

SHEET INDEX	
SHEET No.	SHEET
1	TITLE SHEET
2	ROXBURY ROAD - REALIGNMENT PLAN AND PROFILE
3	DOUBLE BRIDGES COURT - PLAN AND PROFILE
4	ROXBURY ROAD - TRAFFIC CIRCLE PLAN AND DETAILS
5	ROADWAY DETAILS
6	DETOUR PLAN
7	LANDSCAPE PLAN
8	STREET TREE, GRADING AND SEDIMENT CONTROL PLAN
9	STREET TREE, GRADING AND SEDIMENT CONTROL PLAN
10	STORM DRAIN PROFILES & STRUCTURE SCHEDULE
11	STORM DRAIN PROFILES
12	SEDIMENT CONTROL NOTES
13	SEDIMENT CONTROL DETAILS
14	STORMWATER MANAGEMENT PROFILES AND DETAILS - BMP#1
15	STORMWATER MANAGEMENT PROFILES AND DETAILS - BMP#2
16	STORMWATER MANAGEMENT NOTES AND DETAILS
17	SOIL BORINGS AND TEMPORARY TRAFFIC CONTROL
18	DRAINAGE AREA MAP
19	FOREST CONSERVATION PLAN
20	BIO-RETENTION PLAN AND DETAILS

# FINAL ROAD CONSTRUCTION, GRADING AND STORMWATER MANAGEMENT PLAN

# MERIWETHER FARM

## SECTION ONE

**LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
AND NON-BUILDABLE PARCEL 'E'**

**ZONING: RC-DEO**

**TAX MAP No. 21    GRID No. 14, 15, 20 & 21    PARCEL No. 24**

APPROVED: DEPARTMENT OF PLANNING AND ZONING  
*Cindy Harrell* 2/3/09  
 CHIEF, DIVISION OF LAND DEVELOPMENT  
 DATE

APPROVED: HOWARD COUNTY DEPARTMENT OF PUBLIC WORKS  
*Walter D. ...* 2/4/09  
 CHIEF, DEVELOPMENT ENGINEERING DIVISION  
 DATE

APPROVED: BUREAU OF HIGHWAYS  
*Walter D. ...* 1-22-09  
 CHIEF, BUREAU OF HIGHWAYS  
 DATE

- GENERAL NOTES:**
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST STANDARDS AND SPECIFICATIONS OF HOWARD COUNTY PLUS HIGH STANDARDS AND SPECIFICATIONS IF APPLICABLE.
  - THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT OF PUBLIC WORKS, BUREAU OF ENGINEERING, CONSTRUCTION INSPECTION DIVISION AT 4100 315-1800 AT LEAST 10 WORKING DAYS PRIOR TO THE START OF WORK.
  - THE CONTRACTOR SHALL NOTIFY "955 UTILITY" AT 1-800-257-7777 AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION WORK BEING DONE.
  - TRAFFIC CONTROL DEVICES, MARKINGS AND SIGNING SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). ALL STREET AND REGULATORY SIGNS SHALL BE IN PLACE PRIOR TO THE PLACEMENT OF ANY ASPHALT.
  - THE COORDINATES SHOWN HEREON ARE BASED UPON THE HOWARD COUNTY GEODETIC CONTROL WHICH IS BASED UPON THE MARYLAND STATE PLANE COORDINATE SYSTEM, HOWARD COUNTY HORIZONTAL AND VERTICAL CONTROL DATUM IS BASED ON HOWARD COUNTY GEODETIC CONTROL STATIONS:  
 HOWARD COUNTY MONUMENT NO. 21 EA N 582704989 ELEV. + 450292'  
 HOWARD COUNTY MONUMENT NO. 21 E2 N 582706443 ELEV. + 476553'  
 E 1300668484
  - THE TRAFFIC STUDY FOR THIS PROJECT WAS PREPARED BY HARS GROUP DATED NOVEMBER, 2006 AND WAS APPROVED UNDER SP-07-007.
  - BACKGROUND INFORMATION:  
 A. SUBDIVISION NAME: MERIWETHER FARM  
 B. TAX MAP NO.: 21  
 C. PARCEL NO.: 24  
 D. ZONING: RC-DEO  
 E. ELECTION DISTRICT: FOURTH  
 F. TOTAL TRACT AREA: 60.882 AC.  
 G. NET AREA = 60.882 - 6.82 - 5.60 + 48.442 AC.  
 H. AREA OF STEEP SLOPES 25% AND GREATER = 6.07 AC. TOTAL (5.62 AC. OUTSIDE FLOODPLAIN)  
 I. NO. OF BUILDABLE LOTS: 12  
 J. NO. OF NON-BUILDABLE PRESERVATION PARCELS: 3  
 K. NO. OF BUILDABLE BULK PARCELS: 1  
 L. NO. OF NON-BUILDABLE BULK PARCELS: 1  
 M. AREA OF BUILDABLE LOTS: 12.375 AC.  
 N. AREA OF NON-BUILDABLE PRESERVATION PARCELS: 10.91 AC.  
 O. AREA OF BUILDABLE PRESERVATION PARCELS: 34.470 AC.  
 P. AREA OF NON-BUILDABLE BULK PARCELS: 0.93 AC.  
 Q. TOTAL AREA OF ROADWAY TO BE DEDICATED: 2.025 AC.  
 R. PREVIOUS FILE NOS.: SP-07-007  
 S. AREA OF FLOODPLAIN: 6.82 AC.  
 T. NO. CEMETERIES EXIST WITHIN THIS SUBDIVISION.  
 U. ALL FILL AREAS WITHIN ROADWAYS AND UNDER STRUCTURES SHALL BE COMPACTED TO A MINIMUM OF 95% COMPACTION OF AASHO T-100.
  - STORMWATER MANAGEMENT FACILITY WILL BE PROVIDED IN ACCORDANCE WITH HOWARD COUNTY AND MARYLAND STD SPECIFICATIONS. RECHARGE VOLUME WILL BE PROVIDED THRU THE USE OF GRASS CHANNELS, WATERS QUALITY AND CHANNEL PROTECTION VOLUME WILL BE PROVIDED BY ONE MICROPOOL, EXTENDED DETENTION POND & ONE POCKET POND EXTENDED DETENTION POND. OVERSIZING FLOOD PROTECTION VOLUME AND EXTENSIVE FLOOD VOLUME ARE NOT REQUIRED FOR THIS SITE. STORMWATER MANAGEMENT FACILITIES NO. 2 & 3 WILL BE OWNED BY THE HOMEOWNER'S ASSOCIATION AND JOINTLY MAINTAINED BY THE H.O.A. AND HOWARD COUNTY.  
 a. BMP NO. 1 IS A BIO-RETENTION FACILITY AND IS PRIVATELY OWNED AND MAINTAINED LOCATED ON BUILDABLE PRESERVATION PARCEL 'A'.  
 b. BMP NO. 2 IS A MICRO-POOL EXTENDED DETENTION POND THAT IS PRIVATELY OWNED BY H.O.A. AND JOINTLY MAINTAINED BY H.O.A. & HO. CO. LOCATED ON NON-BUILDABLE PRESERVATION PARCEL 'B'.  
 c. BMP NO. 3 IS A POCKET POND EXTENDED DETENTION FACILITY AND IS PRIVATELY OWNED AND JOINTLY MAINTAINED BY H.O.A. & HO. CO. LOCATED ON NON-BUILDABLE PRESERVATION PARCEL 'D'.  
 11. THE PROPOSED WATER AND SEWER SYSTEMS SHALL BE PRIVATE.
  - THE SUBJECT PROPERTY IS LOCATED OUTSIDE OF THE METROPOLITAN DISTRICT.
  - TOPOGRAPHIC CONTOURS BASED ON AERIAL BY HARFORD AERIAL SURVEYS, INC. DATED APRIL 6, 2004.
  - FOR FLAG OR PRESENT LOTS, REFUSE COLLECTION, SNOW REMOVAL AND ROAD MAINTENANCE IS TO BE PROVIDED AT THE JUNCTION OF THE FLAG OR PRESENT AND THE ROAD RIGHT-OF-WAY AND NOT ONTO THE FLAG OR PRESENT LOT.
  - BOUNDARY OUTLINE BASED ON FIELD SURVEY PERFORMED BY FISHER, COLLINS & CARTER, INC. DATED JUNE 15, 2005.
  - SUBJECT PROPERTY ZONED RC-DEO PER THE 2004 ZONING REGULATIONS AND THE COM-PLITE ZONING AMENDMENTS DATED 7/22/06.
  - THIS SUBDIVISION PLAN IS SUBJECT TO THE AMENDED FIFTH EDITION OF THE SUBDIVISION AND LAND DEVELOPMENT REGULATIONS AND THE 2004 ZONING REGULATIONS PER COUNCIL BILL NO. 45-2005 AND THE ZONING REGULATIONS AS AMENDED BY COUNCIL BILL NO. 75-2003. DEVELOPMENT OR CONSTRUCTION ON THIS LOTS OR PARCELS MUST COMPLY WITH SEPARATE AND DIFFERENT REGULATIONS IN EFFECT AT THE TIME OF SUBMISSION OF A BUILDING OR GRADING PERMIT APPLICATION.
  - THERE ARE AREAS OF STEEP SLOPES (25% OR GREATER) LOCATED ON THIS PROPERTY AS DEFINED BY THE HOWARD COUNTY SUBDIVISION AND LAND DEVELOPMENT REGULATIONS, SECTION 16.03. THESE AREAS ARE LOCATED WITHIN THE PRESERVATION PARCELS AND NOT ON THE RESIDENTIAL LOTS.
  - THIS AREA DESIGNATES A PRIVATE SEWERAGE EASEMENT OF 10,000 SQUARE FEET AS REQUIRED BY THE MARYLAND STATE DEPARTMENT OF THE ENVIRONMENT FOR INDIVIDUAL SEWAGE DISPOSAL IMPROVEMENTS OF ANY NATURE IN THIS AREA ARE RESTRICTED UNTIL PUBLIC SEWERAGE IS AVAILABLE. THESE EASEMENTS SHALL BECOME NULL AND VOID UPON CONNECTION TO A PUBLIC SEWERAGE SYSTEM. THE COUNTY HEALTH OFFICER SHALL HAVE THE AUTHORITY TO GRANT ADJUSTMENTS TO THE PRIVATE SEWERAGE EASEMENT. RECORDATION OF A MODIFIED EASEMENT SHALL NOT BE NECESSARY.
  - THE NON-CRITICAL FLOODPLAIN STUDY FOR THIS PROJECT WAS PREPARED BY FISHER, COLLINS & CARTER, INC. DATED NOVEMBER, 2006, AND SUPPLEMENTED WITH INFORMATION OBTAINED FROM MDCO. CAPITAL PROJECT D-1075 AND WAS APPROVED UNDER SP-07-007.
  - AS PER SECTION 10A.4.B OF THE ZONING REGULATIONS, ONLY ONE EASEMENT HOLDER IS REQUIRED FOR PRESERVATION PARCELS DESIGNATED SOLELY FOR SMH FACILITIES OR COMMUNITY SEWERAGE DISPOSAL.  
 A. BUILDABLE PRESERVATION PARCEL 'A' OWNED PRIVATELY OWNED USE: ENVIRONMENTAL PROTECTION  
 EASEMENT HOLDERS: HOMEOWNER'S ASSOCIATION AND HOWARD COUNTY, MARYLAND  
 B. NON-BUILDABLE PRESERVATION PARCEL 'B' OWNED HOMEOWNER'S ASSOCIATION USE: ENVIRONMENTAL PROTECTION  
 EASEMENT HOLDER: HOWARD COUNTY, MARYLAND  
 C. NON-BUILDABLE PRESERVATION PARCEL 'C' OWNED HOMEOWNER'S ASSOCIATION USE: ENVIRONMENTAL PROTECTION  
 EASEMENT HOLDERS: HOMEOWNER'S ASSOCIATION AND HOWARD COUNTY, MARYLAND  
 D. NON-BUILDABLE PRESERVATION PARCEL 'D' OWNED HOMEOWNER'S ASSOCIATION USE: ENVIRONMENTAL PROTECTION  
 EASEMENT HOLDER: HOWARD COUNTY, MARYLAND AND USE: S.W.H.
  - NO CLEARING, GRADING OR CONSTRUCTION IS PERMITTED WITHIN THE WETLANDS, STREAM, FOREST CONSERVATION EASEMENT OR THEIR REQUIRED BUFFERS.
  - THE GEOTECHNICAL REPORT FOR THIS PROJECT WAS PREPARED BY PENNMAN & BROWN DATED NOVEMBER, 2006.
  - THE FOREST STAND DELINEATION AND WETLAND DELINEATION FOR THIS PROJECT WAS PREPARED BY ECO-SCIENCE PROFESSIONALS, INC. DATED NOVEMBER, 2006 AND WAS APPROVED UNDER SP-07-007.
  - THE FOREST CONSERVATION REQUIREMENTS PER SECTION 16.1200 OF THE HOWARD COUNTY CODE AND THE FOREST CONSERVATION MANUAL FOR THIS SUBDIVISION WILL BE FULFILLED BY PROVIDING 10.0 AC. OF FOREST RETENTION IN AN FOREST CONSERVATION EASEMENT.  
 THE FOREST CONSERVATION SURETY AMOUNT REQUIRED IS \$62,043.20 AND WILL BE PROVIDED WITH THE DEVELOPER'S AGREEMENT.
  - THE LANDSCAPE SURETY IN THE AMOUNT OF \$50,000.00 FOR PRESERVE LANDSCAPE REQUIREMENTS IS 5% SHADY TREES AND 4% EVERGREEN TREES) OF SECTION 16.124 OF THE HOWARD COUNTY CODE AND LANDSCAPE MANUAL IS POSTED WITH THE DEVELOPER'S AGREEMENT FOR THIS SUBDIVISION.
  - FINANCIAL SURETY FOR THE 64 REQUIRED STREET TREES HAS BEEN POSTED AS PART OF THE DEVELOPER'S AGREEMENT IN THE AMOUNT OF \$93,200.00.
  - ALL EXISTING TRENCHES AND SEPTIC FIELDS WITHIN 100 FEET OF SUBJECT PROPERTY HAVE BEEN SHOWN.
  - SIGN POSTS: ALL SIGN POST USED FOR TRAFFIC CONTROL SIGNS INSTALLED IN THE COUNTY RIGHT-OF-WAY SHALL BE MOUNTED ON 4" GALVANIZED STEEL PERFORATED SQUARE TUBE POST (3/4" DIA) INSERTED INTO A 2-1/2" GALVANIZED STEEL PERFORATED SQUARE TUBE SLEEVE (2" DIA) - 3' LONG. A GALVANIZED STEEL POLE CAP SHALL BE MOUNTED ON TOP OF EACH POST.
  - DEWEYWAY (S) SHALL BE PROVIDED PRIOR TO RESIDENTIAL OCCUPANCY TO ENSURE SAFE ACCESS FOR FIRE AND EMERGENCY VEHICLES PER THE FOLLOWING MINIMUM REQUIREMENTS:  
 a) WIDTH - 12 FEET (10 FEET SERVING MORE THAN ONE RESIDENCE)  
 b) SURFACE - SIX (6) INCHES OF COMPACTED CRUSHER RUN BASE WITH TAR AND CHIP COATING.  
 c) GEOMETRY - MINIMUM 10% GRADE, MAXIMUM 10% GRADE CHANGE AND MINIMUM OF 45 FOOT TURNING RADIUS.  
 d) STRUCTURES (CULVERTS/BRIDGES) CAPABLE OF SUPPORTING 25 GROSS TONS (NETS LOADING)  
 e) DRAINAGE ELEVATIONS - CAPABLE OF PASSING 100 YEAR FLOOD WITH NO MORE THAN 1 FOOT DEPTH OVER DREWEYWAY SURFACE.  
 f) STRUCTURE CLEARANCES - MINIMUM 12 FEET  
 g) MAINTENANCE - SUFFICIENT TO ENSURE ALL WEATHER USE.
  - GROUND WATER APPROPRIATION PERMIT NO. 15 H030056209 001.
  - ALL WELLS TO BE BORED PRIOR TO FINAL PLAT APPROVAL. IT IS THE DEVELOPER'S RESPONSIBILITY TO SCHEDULE THE WELL DRILLING PRIOR TO FINAL PLAT SUBMISSION. IT WILL NOT BE CONSIDERED "GOVERNMENT DELAY" IF THE WELL DRILLING HOLDS UP THE HEALTH DEPARTMENT SIGNATURE OF THE SECOND PLAT.
  - THIS PROJECT IS PROVIDING A 50' WIDE STEEP NON-BUILDABLE PARCEL 'E' TO BE CONVEYED TO ROBERT BRUCE OR ASSIGNS AT NO COST TO DEVELOPER FOR ENGINEERING, CONVEYANCE, BUILDING OR INSTALLATION OF A ROAD DEVELOPER SERVED ALL RIGHTS TO INSTALL AND SUBSEQUENTLY PROVIDE EASEMENTS ON OR THRU THE RIGHT-OF-WAY FOR THE BENEFIT OF THE SUBDIVISION OF MERIWETHER FARM, SECTION ONE. DECISION WILL NOT BE REQUIRED IF ALTERNATIVE MEANS OF ACCESS ARE ACQUIRED PRIOR TO RECORD PLAT.
  - IF THERE IS AN EXISTING DWELLING/STRUCTURES ON BUILDABLE PRESERVATION PARCEL 'A' TO REMAIN NO NEW BUILDINGS, EXTENSIONS OR ADDITIONS TO THE EXISTING DWELLING ARE TO BE CONSTRUCTED AT A DISTANCE LESS THAN THE ZONING REGULATION REQUIREMENTS. THE EXISTING WAGON WHEEL AND HOG PEN WILL REMAIN WITH THE UNDERSTANDING THAT IF RECREATION & PAVES DEPENDS DURING THIS INSPECTION THAT THE STRUCTURES SHOULD BE REMOVED BECAUSE THEY DO NOT MEET THE INTENT OF THE FOREST CONSERVATION EASEMENT. IN THIS CASE, UPON REMOVAL OF THE BUILDINGS, SUPPLEMENTAL PLANTING WILL BE PROVIDED.

STORMWATER MANAGEMENT PRACTICES		
LOT No.	DISCONNECTION OF ROOFTOP RUNOFF N-1 (Y/N)	MICRO BIO-RETENTION M-6 (NUMBER)
8	(Y)	
9	(Y)	
10	(Y)	
11	(Y)	
12	(Y)	
Par. A	(Y)	BMP #1

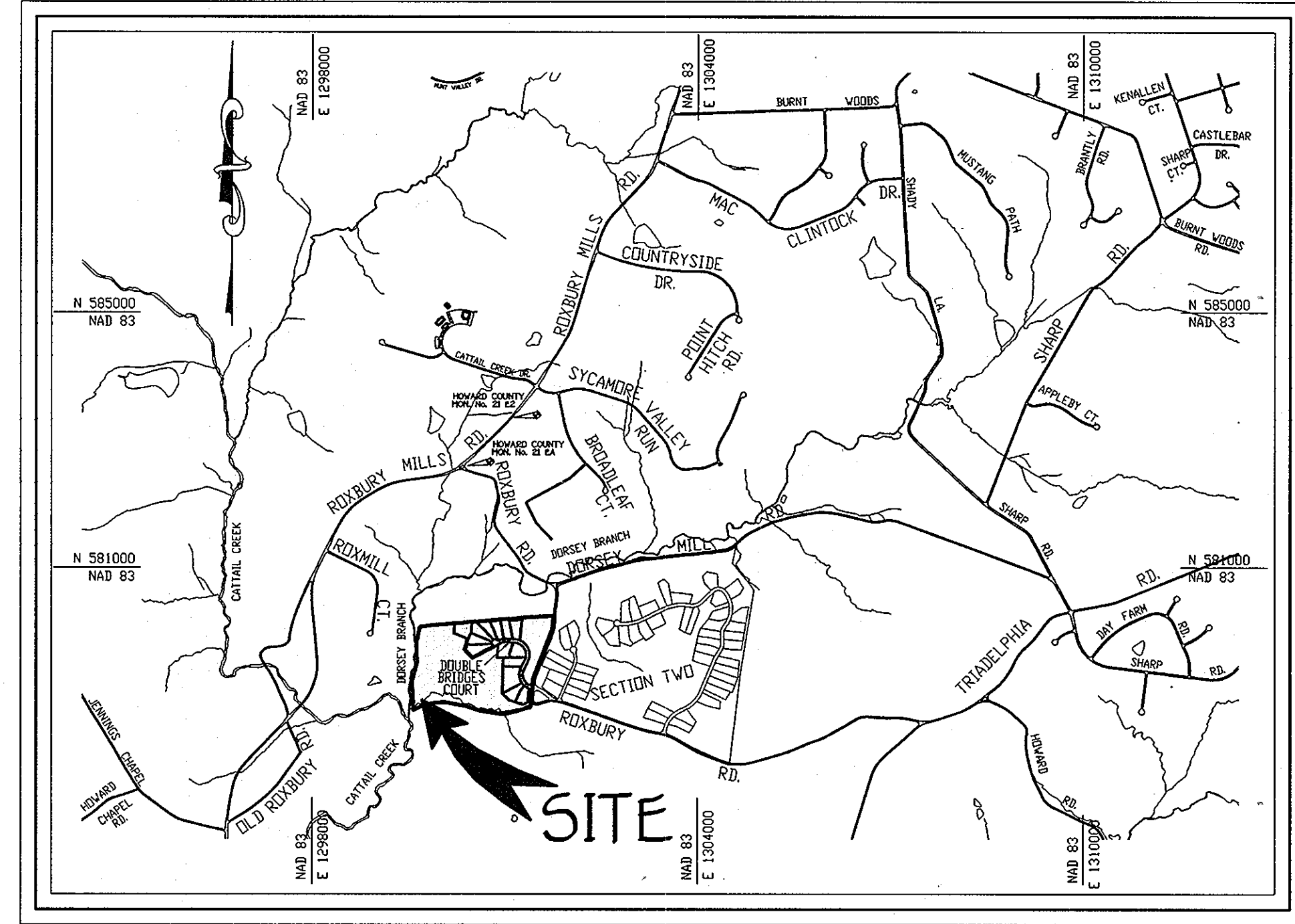
ROADWAY INFORMATION CHART			
ROAD NAME	CLASSIFICATION	DESIGN SPEED	R/W WIDTH
DOUBLE BRIDGES COURT	PUBLIC ACCESS PLACE	25 MPH	50'
REALIGNED ROXBURY ROAD	LOCAL ROAD	30 MPH	50'

TRAFFIC CONTROL SIGNS				
ROAD NAME	CENTERLINE STA.	OFFSET	POSTED SIGN	SIGN CODE
DOUBLE BRIDGES COURT	1+75	13' E	SPEED LIMIT 25	R2-1
DOUBLE BRIDGES COURT	4+50	13' E	SHARP LEFT TURN W/5 MPH SPEED PLATE	W-11
ROXBURY ROAD	30+50	23' L	SPEED LIMIT 30	R2-1
ROXBURY ROAD	37+50	23' E	SPEED LIMIT 30	R2-1

NOTES: 1. SEE SHEET 4 FOR TYPICAL SIGN LAYOUT FOR PROPOSED TRAFFIC CIRCLE.  
 2. NO OUTLET SIGN TO BE PLACED ON DOUBLE BRIDGES COURT STREET SIGN POLE.  
 3. SEE SHEET 2 & 3 FOR ADDITIONAL SIGN LOCATIONS RELATED TO PROPOSED TRAFFIC CIRCLE.

STREET LIGHT CHART			
STREET NAME	C.L. STATION	OFFSET	FIXTURE/POLE TYPE
ROXBURY ROAD ROUNDABOUT	---	---	150-WATT H.P.S. "PREMIER" PENDANT POST-TOP MOUNTED ON A 14-FOOT BLACK FIBERGLASS POLE
ROXBURY ROAD ROUNDABOUT	---	---	150-WATT H.P.S. "PREMIER" PENDANT POST-TOP MOUNTED ON A 14-FOOT BLACK FIBERGLASS POLE
ROXBURY ROAD ROUNDABOUT	---	---	150-WATT H.P.S. "PREMIER" PENDANT POST-TOP MOUNTED ON A 14-FOOT BLACK FIBERGLASS POLE
ROXBURY ROAD ROUNDABOUT	---	---	150-WATT H.P.S. "PREMIER" PENDANT POST-TOP MOUNTED ON A 14-FOOT BLACK FIBERGLASS POLE

SEE SHEET 4 FOR LOCATIONS.



VICINITY MAP  
SCALE: 1" = 2000'

# FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND

ADDED STORMWATER MANAGEMENT CHART		
NO.	DESCRIPTION	DATE
	REVISIONS	

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTRAL SQUARE OFFICE PARK - 10272 BALTIMORE NATIONAL PIKE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 481 - 2855

**OWNER**  
 FRANCIS B. DEVLIN  
 45 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 19950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
 (410) 489-7900



*Walter D. Vitucci*  
 ALDO M. VITUCCI, P.E.  
 DATE: 12-22-09  
 ZONED: RC-DEO  
 TAX MAP No. 21    GRID No. 14, 15, 20 & 21    PARCEL No. 24  
 FOURTH ELECTION DISTRICT, HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 1 OF 20

AS BUILT

CURVE DATA  
**ROXBURY ROAD**  
 STA. 28+25.67 TO STA. 29+74.64  
 RAD. = 350.00'  
 ARC LENGTH = 140.97'  
 TAN. = 75.63'  
 $\Delta = 24^{\circ}23'14''$   
 CHORD. = N 63°10'54" W, 147.85'

CURVE DATA  
**ROXBURY ROAD**  
 STA. 31+05.11 TO STA. 33+06.01  
 RAD. = 350.00'  
 ARC LENGTH = 200.90'  
 TAN. = 103.30'  
 $\Delta = 32^{\circ}53'39''$   
 CHORD. = N 67°25'55" W, 198.16'

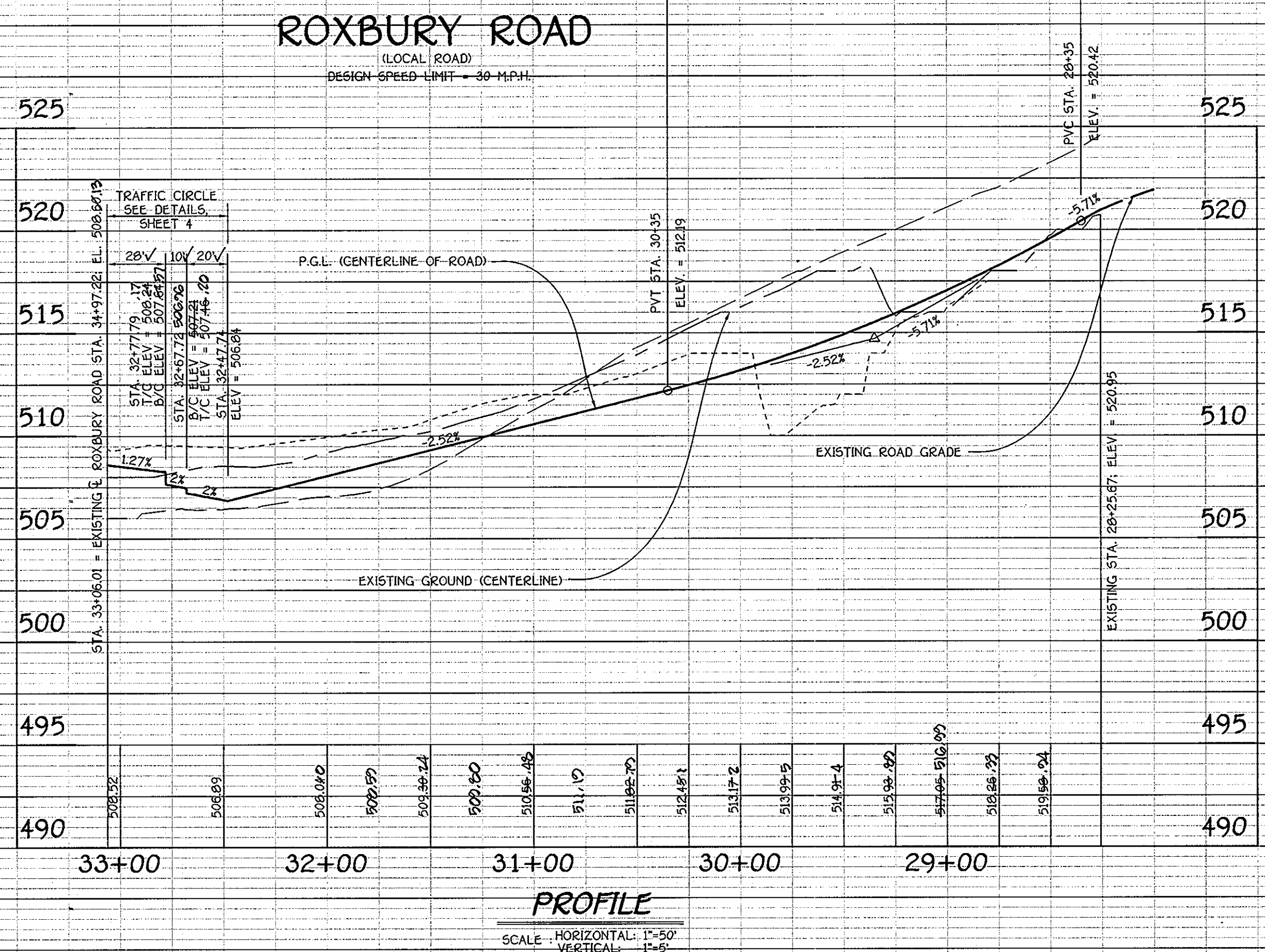
NO.	DESCRIPTION	DATE
REVISIONS		
APPROVED: DEPARTMENT OF PLANNING AND ZONING		
	<i>C. Hammer</i> CHIEF, DIVISION OF LAND DEVELOPMENT	2/5/09 DATE
	<i>W. D. ...</i> CHIEF, DEVELOPMENT ENGINEERING DIVISION	2/14/09 DATE
APPROVED: HOWARD COUNTY DEPARTMENT OF PUBLIC WORKS		
	<i>W. Z. ...</i> CHIEF, BUREAU OF HIGHWAYS	1-22-09 DATE

**NOTE:**  
 THIS PROJECT IS PROVIDING A 50' WIDE STRIP NON-BUILDABLE PARCEL 'E' TO BE CONVEYED TO ROBERT BUICE OR ASSIGNS AT NO COST TO DEVELOPER FOR ENGINEERING CONVEYANCE, BUILDING OR INSTALLATION OF A ROAD DEVELOPER RESERVES ALL RIGHTS TO RESUBMIT AND SUBSEQUENTLY PROVIDE EASEMENTS ON OR THRU THE RIGHT-OF-WAY FOR THE BENEFIT OF THE SUBDIVISION OF MERIWETHER FARM SECTION ONE. DEDICATION WILL NOT BE REQUIRED IF ALTERNATIVE MEANS OF ACCESS ARE ACQUIRED PRIOR TO RECORD PLAT.

**NOTE:**  
 THE EXISTING PAVING TO REMAIN IS TO SERVE LOT 2, BUICE PROPERTY, PLAT Nos. 5426-5429 & LOT 3, CHASE FARM, PLAT No. 12067 FOR ANY FUTURE DEVELOPMENT.

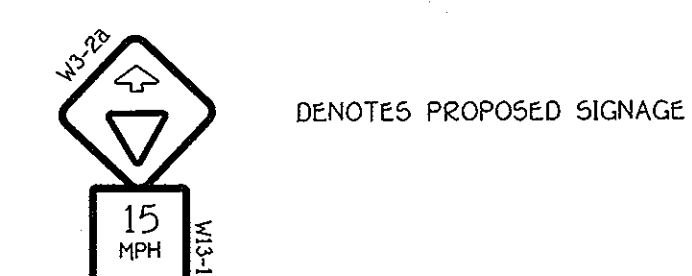
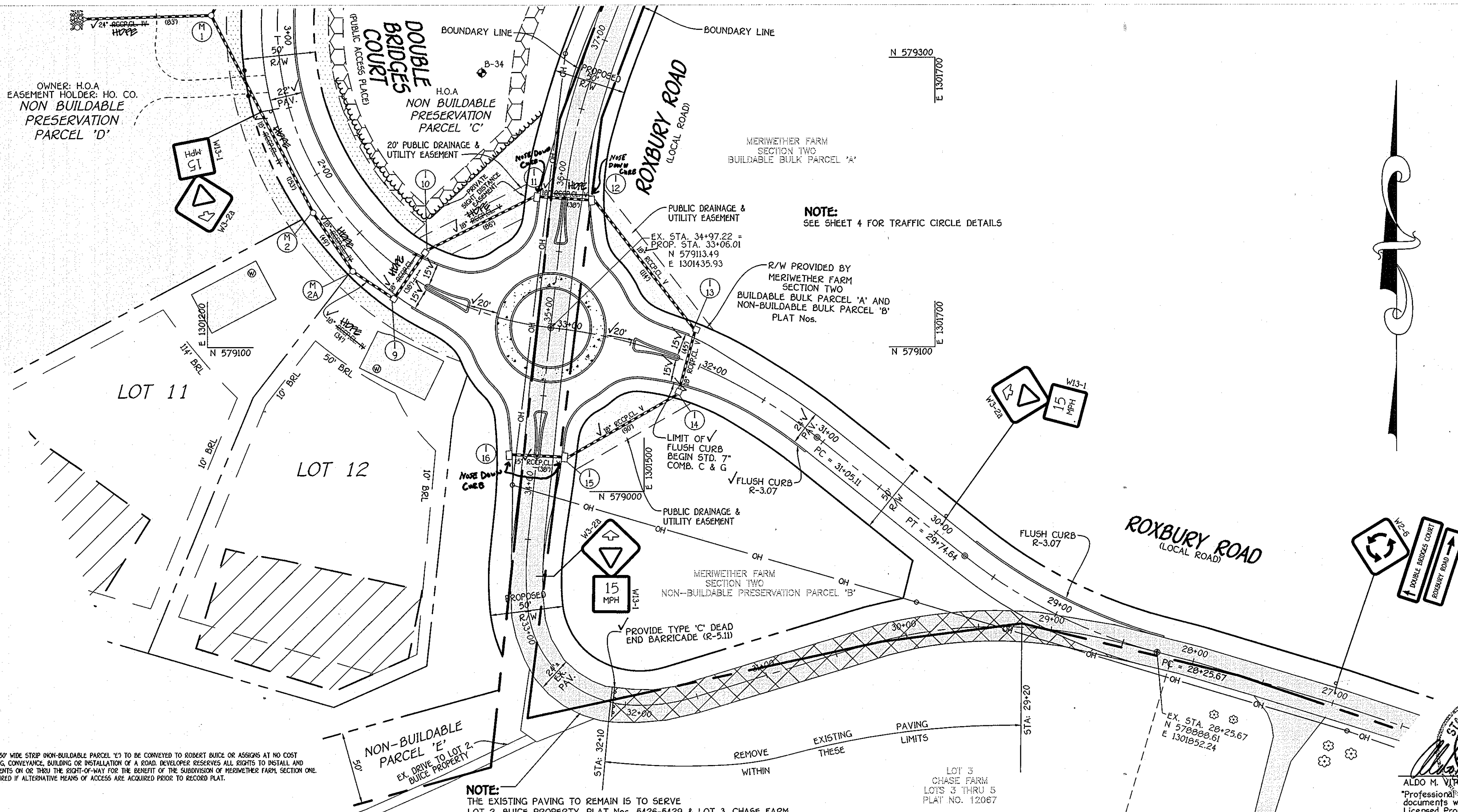
**VERTICAL CURVE DATA**

PVI STA = 29+35  
 PVI-ELEV = 514.71  
 CORSE = +0.92'  
 HSD REQ'D = 200'  
 HSD PROV'D = 360'  
 200' VC



**PROFILE**

SCALE: HORIZONTAL: 1"=50'  
VERTICAL: 1"=5'



DENOTES PROPOSED SIGNAGE



**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'

TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND

**ROXBURY ROAD**  
 PLAN AND PROFILE

<b>OWNER</b>	<b>DEVELOPER</b>
FRANCES B. DEVLIN 43 LITCHFIELD ROAD LITCHFIELD, CT. 06759	HERITAGE LAND DEVELOPMENT 15950 NORTH AVENUE, P.O. BOX 482 LISBON, MARYLAND 21765 (410) 489-7900

SCALE: AS SHOWN DATE: DEC. 17, 2008 DWG. NO. 2 OF 20  
 DES. R.A.I. DRN. J.C.L. CHK. A.M.V.

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 10272 MALTBORE NATIONAL PIKE  
 ELKLOTT CITY, MARYLAND 2042  
 410-481-2822

F-08-139  
AS BUILT

CURVE DATA  
**DOUBLE BRIDGES COURT**  
 STA. 0+00.00 TO STA. 4+04.66  
 RAD. = 210.00'  
 ARC LENGTH = 404.66'  
 TAN. = 302.19'  
 $\Delta = 110^{\circ}24'27''$   
 CHORD. = N 28^{\circ}40'22" W, 344.90'

CURVE DATA  
**DOUBLE BRIDGES COURT**  
 STA. 5+43.29 TO STA. 9+57.29  
 RAD. = 210.00'  
 ARC LENGTH = 414.00'  
 TAN. = 317.00'  
 $\Delta = 112^{\circ}57'16''$   
 CHORD. = N 29^{\circ}56'47" W, 350.14'

CURVE DATA  
**DOUBLE BRIDGES COURT**  
 STA. 10+95.52 TO STA. 12+02.14  
 RAD. = 295.00'  
 ARC LENGTH = 106.63'  
 TAN. = 53.94'  
 $\Delta = 21^{\circ}28'09''$   
 CHORD. = S 62^{\circ}51'30" W, 106.01'

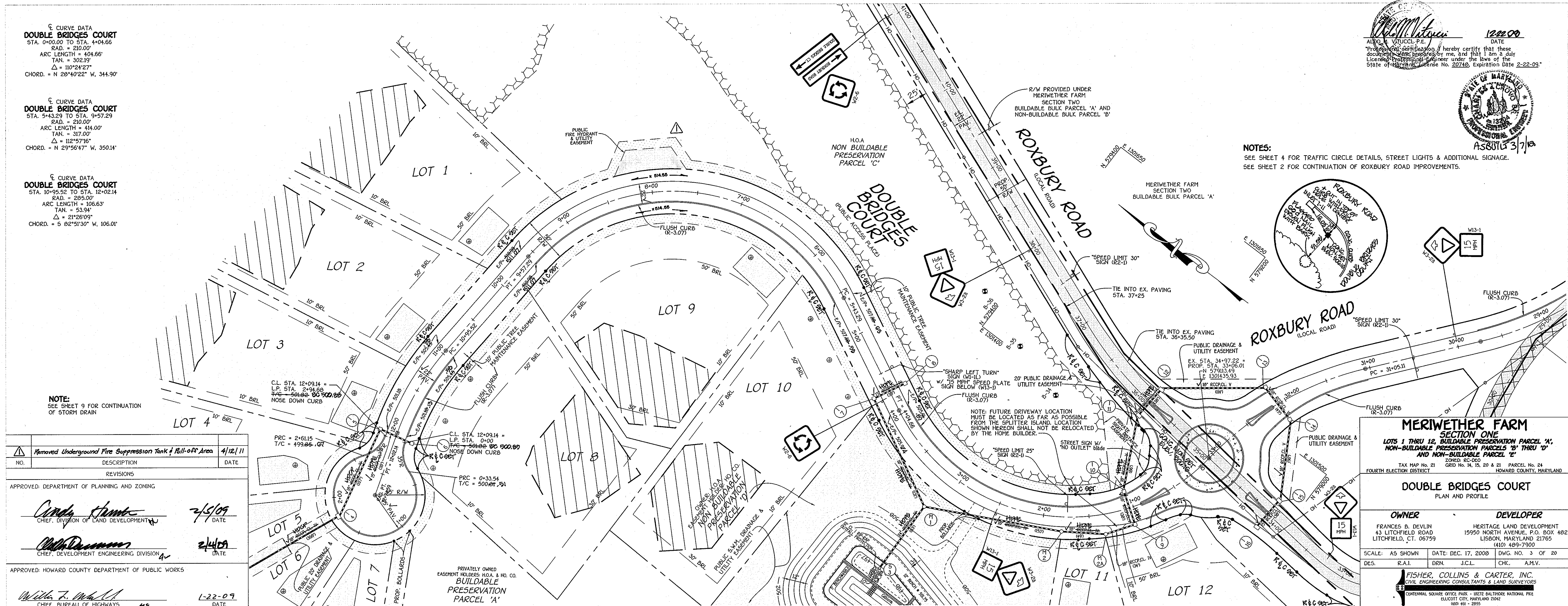
NOTE:  
 SEE SHEET 9 FOR CONTINUATION  
 OF STORM DRAIN

NO.	DESCRIPTION	DATE
1	Removed Underground Fire Suppression Tank & Full-off Area	4/12/11
REVISIONS		
APPROVED: DEPARTMENT OF PLANNING AND ZONING		
<i>Clayton Shanks</i>		2/5/09 DATE
CHIEF, DIVISION OF LAND DEVELOPMENT		
<i>Clayton Shanks</i>		2/4/09 DATE
CHIEF, DEVELOPMENT ENGINEERING DIVISION		
APPROVED: HOWARD COUNTY DEPARTMENT OF PUBLIC WORKS		
<i>William Z. ...</i>		1-22-09 DATE
CHIEF, BUREAU OF HIGHWAYS		

PROFESSIONAL CERTIFICATION: I hereby certify that these documents were prepared by me, and that I am a duly licensed Professional Engineer under the laws of the State of Maryland, License No. 20740, Expiration Date 2-22-09.

DATE: 12-22-09  
 ALDO K. VITUCI, P.E.

**NOTES:**  
 SEE SHEET 4 FOR TRAFFIC CIRCLE DETAILS, STREET LIGHTS & ADDITIONAL SIGNAGE.  
 SEE SHEET 2 FOR CONTINUATION OF ROXBURY ROAD IMPROVEMENTS.



**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D',  
 AND NON-BUILDABLE PARCEL 'E'

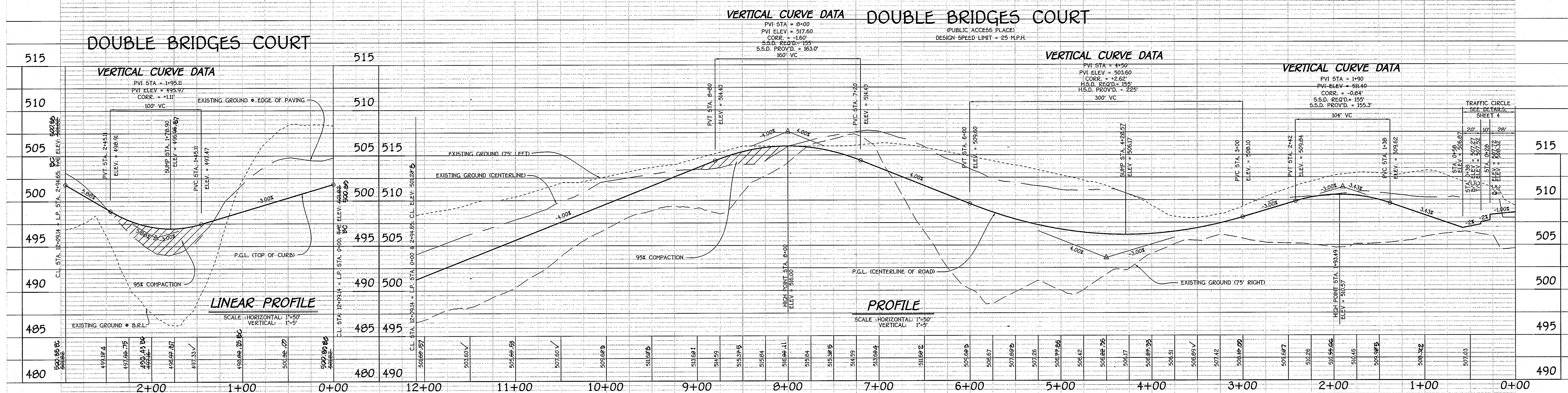
TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND

**DOUBLE BRIDGES COURT**  
 PLAN AND PROFILE

OWNER	DEVELOPER
FRANCES B. DEVLIN 43 LITCHFIELD ROAD LITCHFIELD, CT. 06759	HERITAGE LAND DEVELOPMENT 15950 NORTH AVENUE, P.O. BOX 482 LISBON, MARYLAND 21765 (410) 489-7900

SCALE: AS SHOWN DATE: DEC. 17, 2008 DWG. NO. 3 OF 20  
 DES. R.A.I. DRN. J.C.L. CHK. A.M.V.

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 1072 BALTIMORE NATIONAL FREE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 461-2055

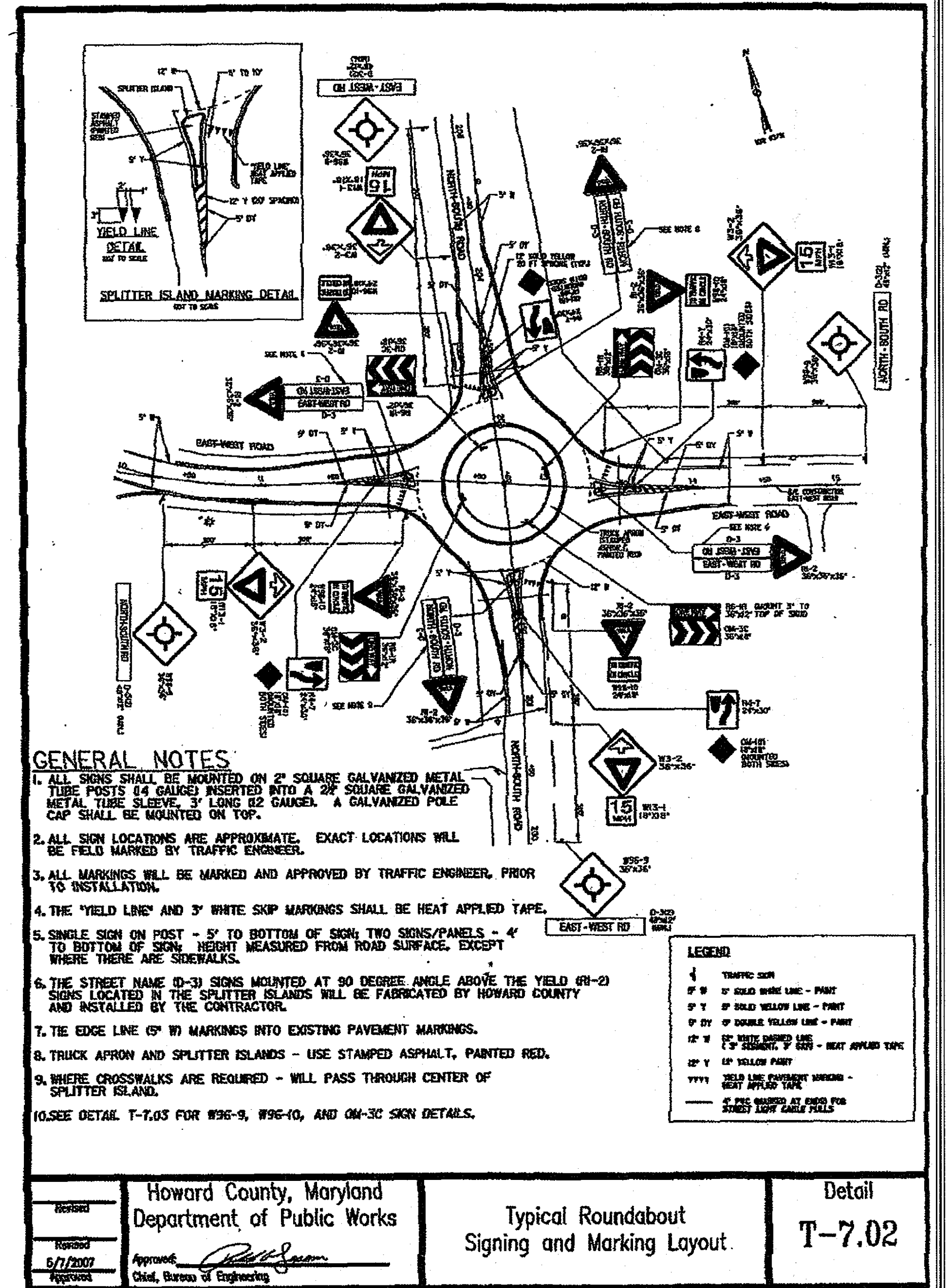
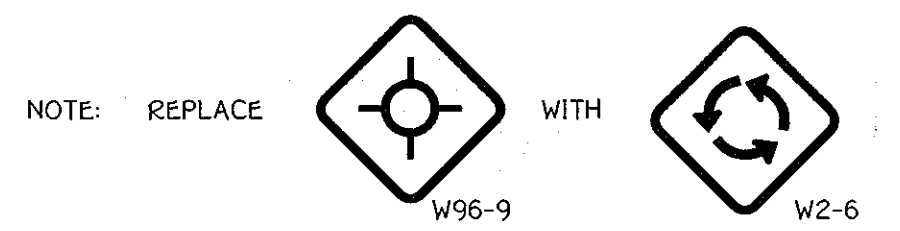
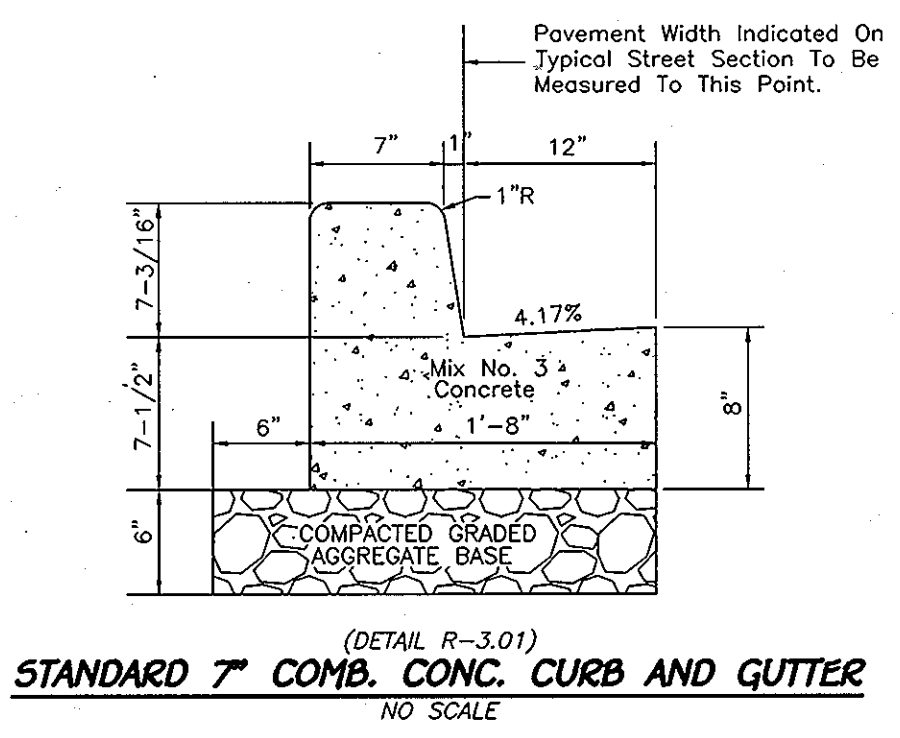
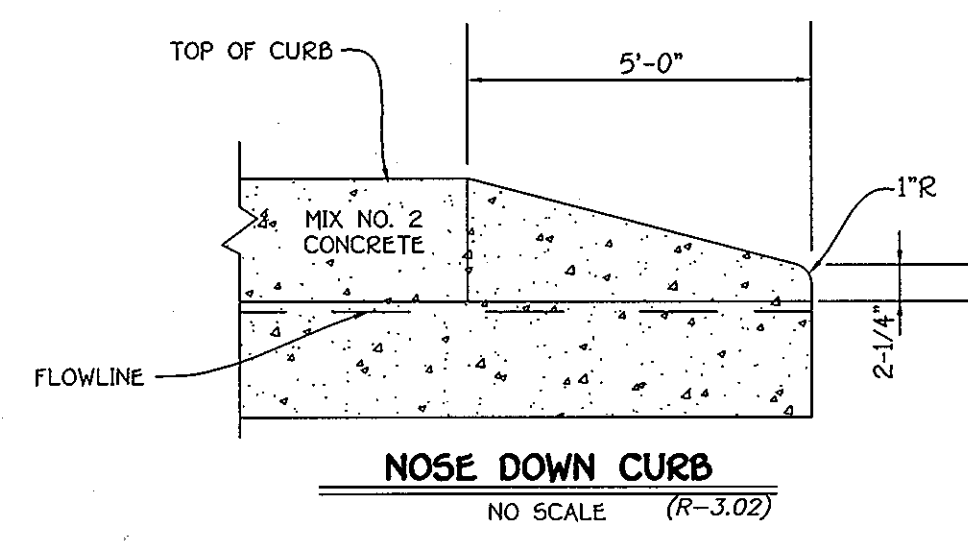
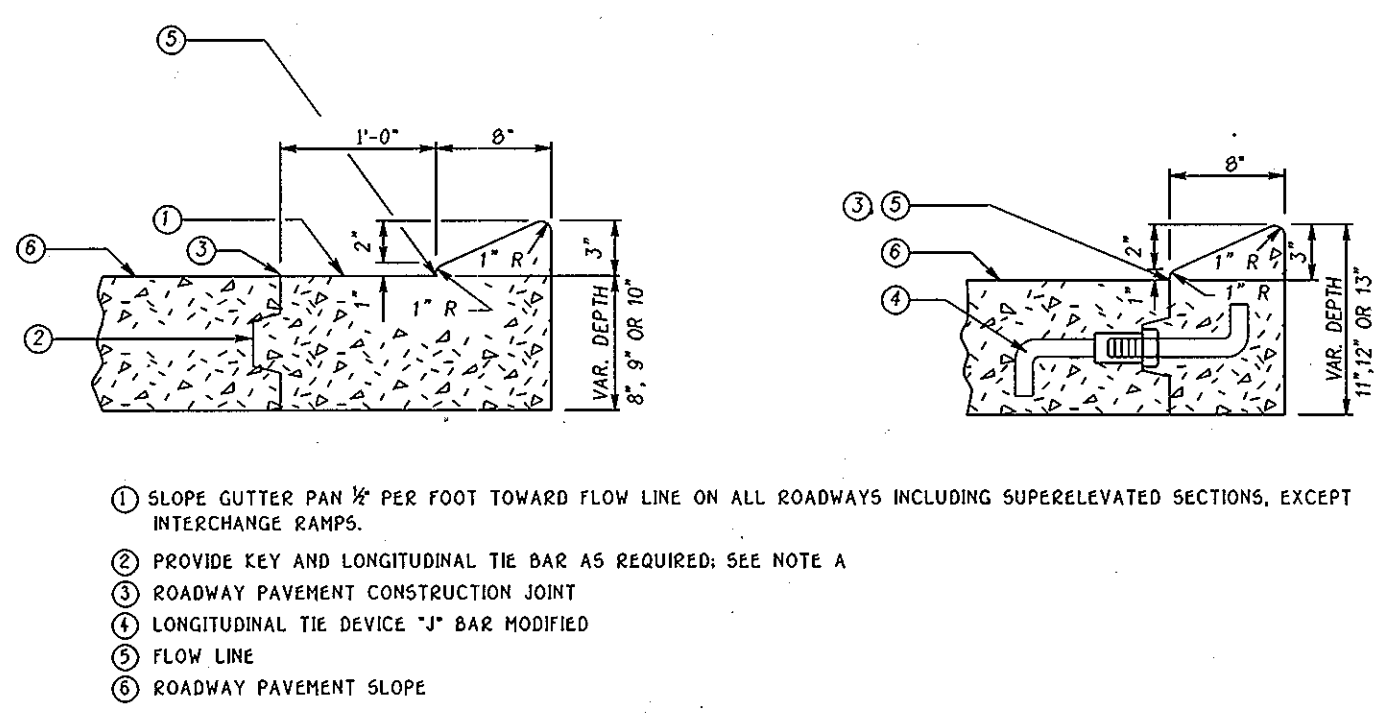
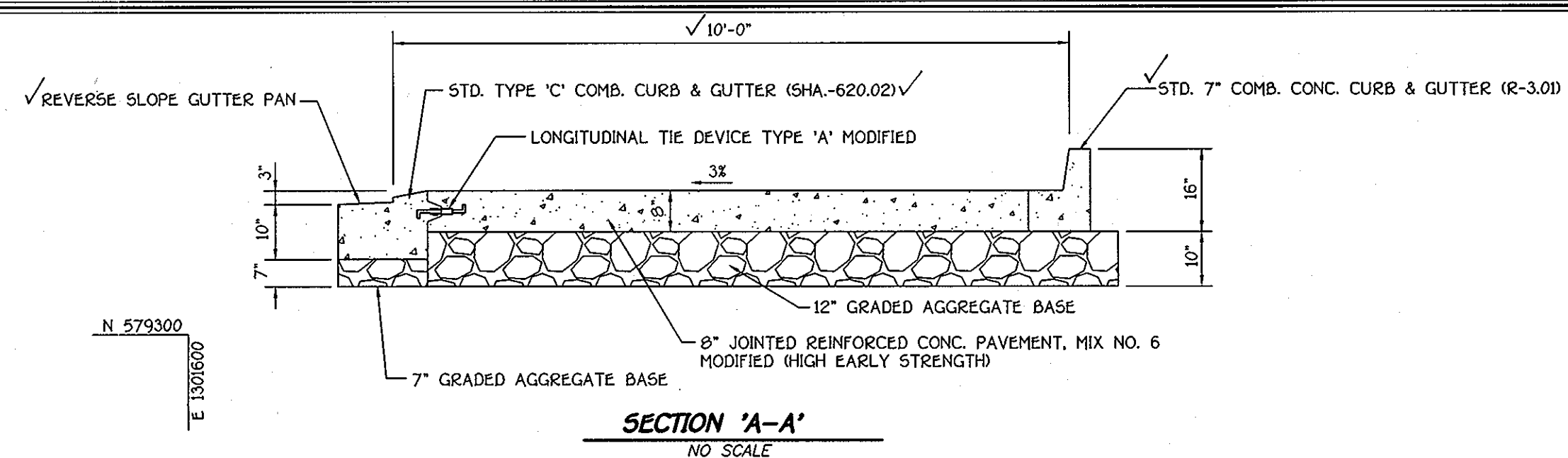
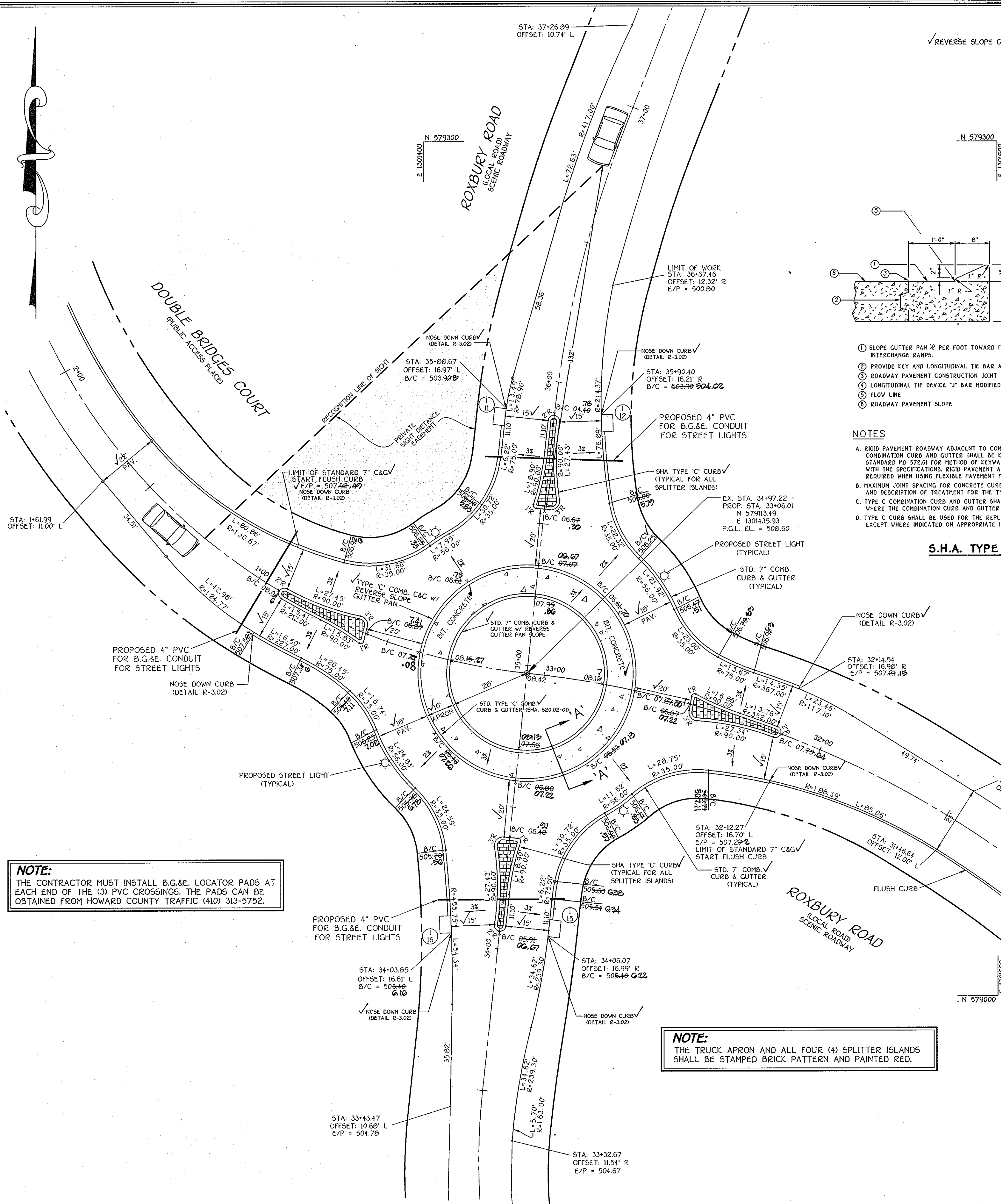


F-08-139  
AS BUILT

Approved Department of Public Works  
 Chief Bureau of Highways  
 Date: 1-22-09

Approved Department of Planning and Zoning  
 Chief, Division of Land Development  
 Date: 2/5/09

Chief, Development Engineering Division  
 Date: 2/4/09



**ROUNDABOUT DETAIL**

SCALE: 1" = 20'

**OWNER**  
 FRANCIS B. DEVLIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 19950 NORTH AVENUE, P.O. BOX 402  
 LISBON, MARYLAND 21765  
 (410) 489-7900

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL, ENGINEERING, CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 10272 BALTIMORE NATIONAL PIKE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 481-2355

**Professional Engineer**  
 ALDO M. VITALE  
 DATE: 12/22/09



**TRAFFIC CIRCLE PLAN AND DETAILS**  
**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'

TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 4 OF 20

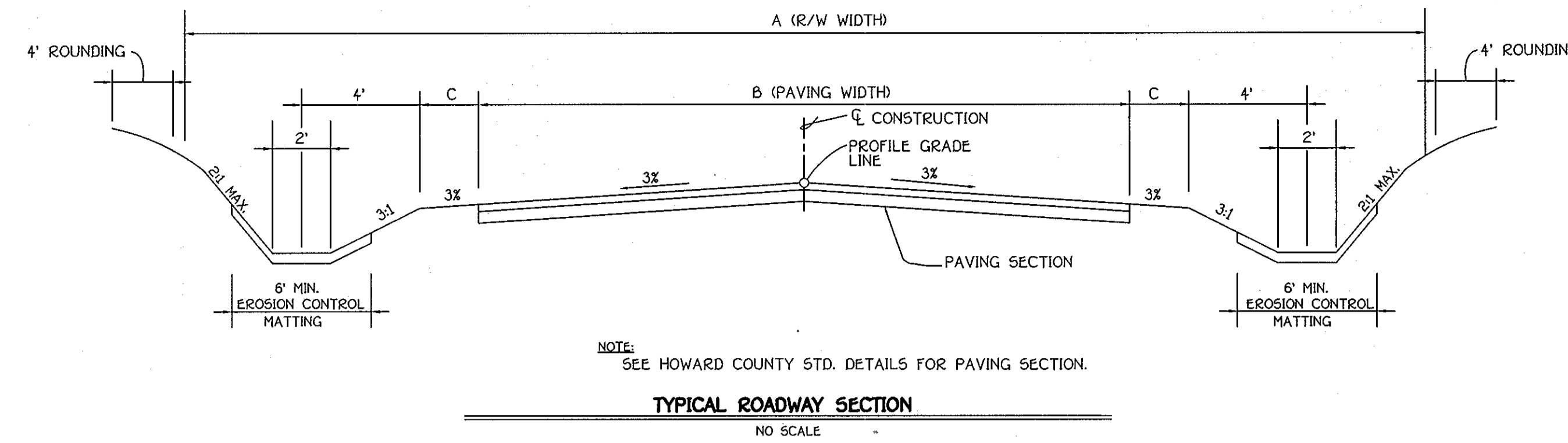
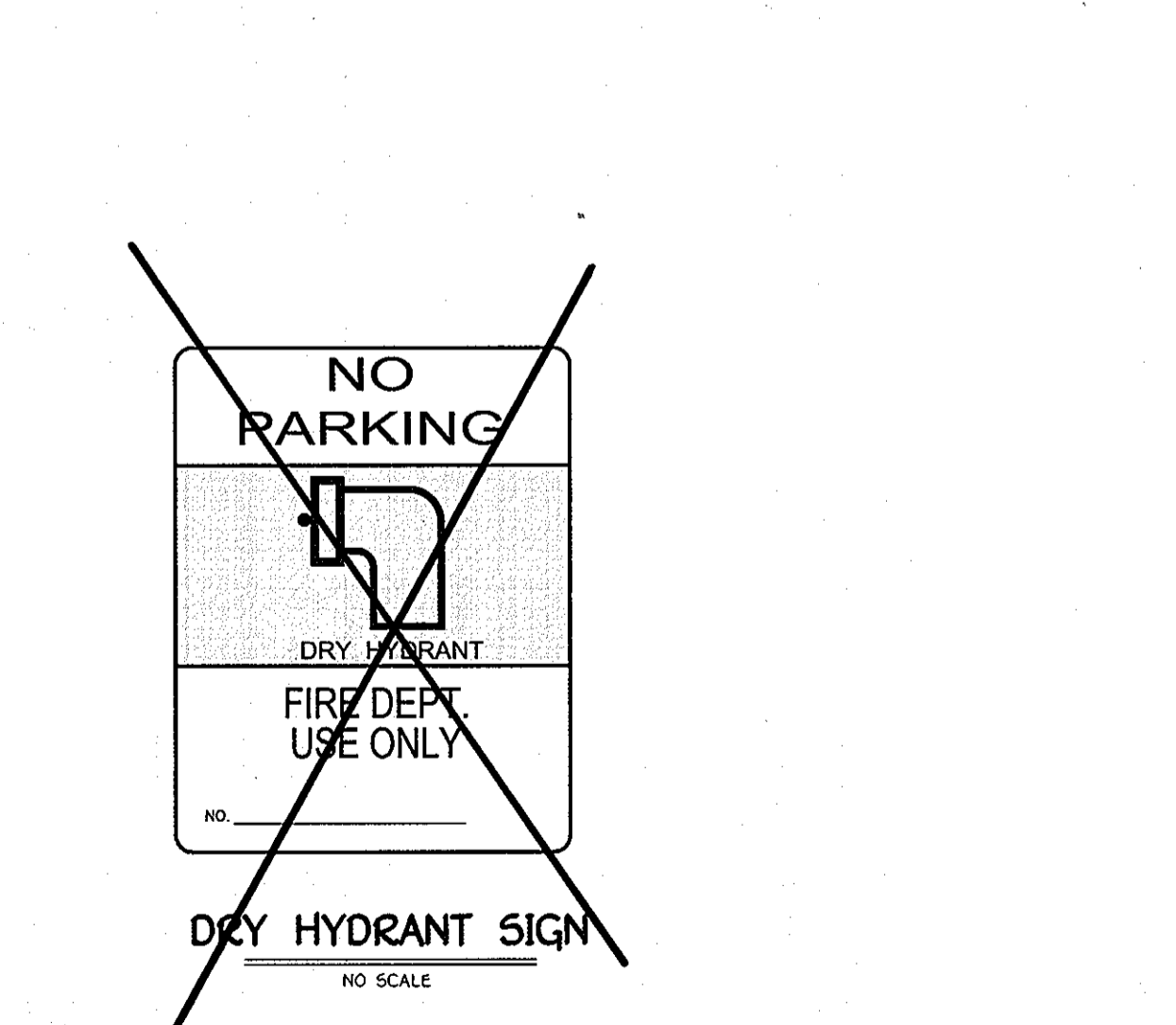
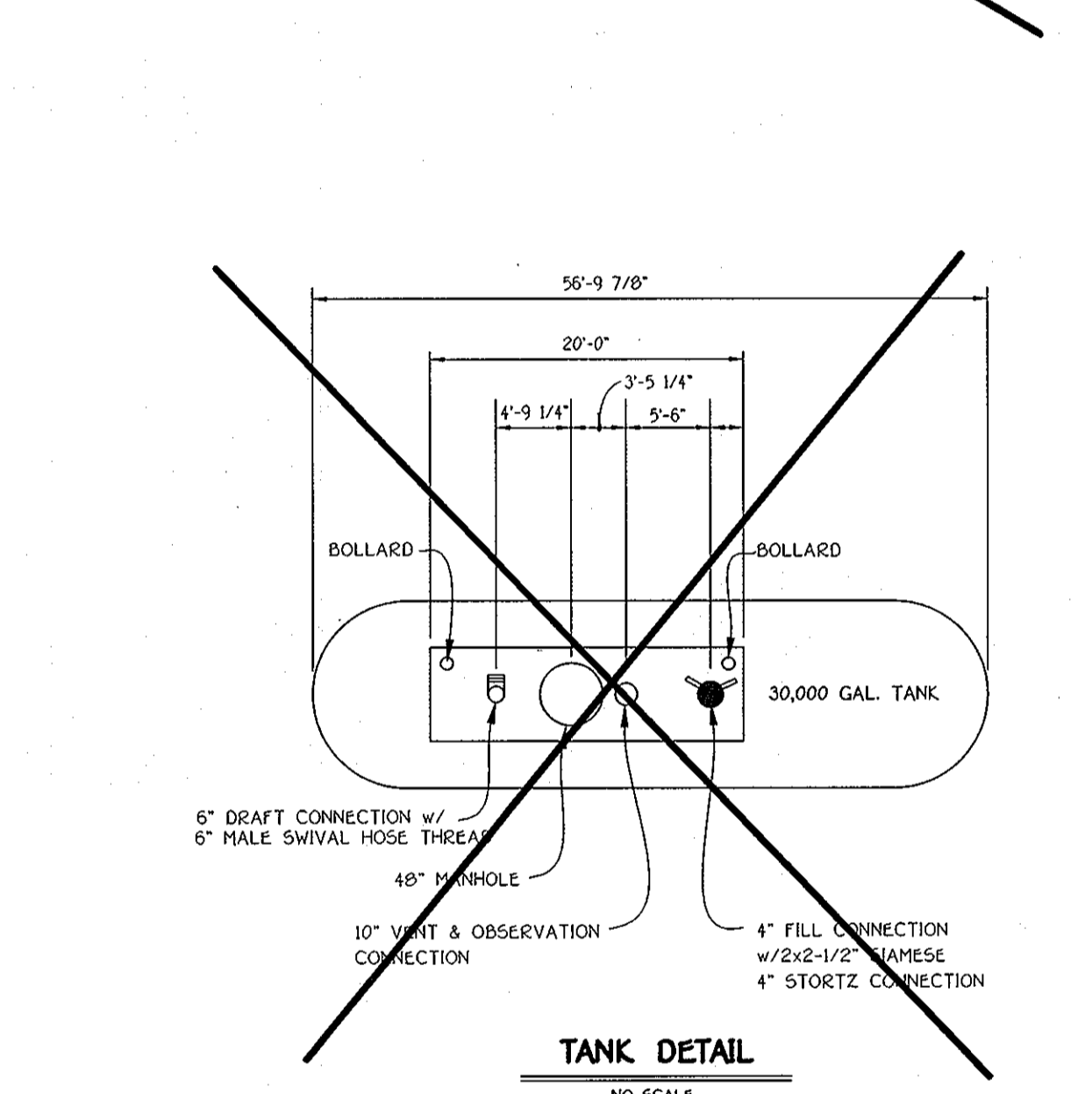
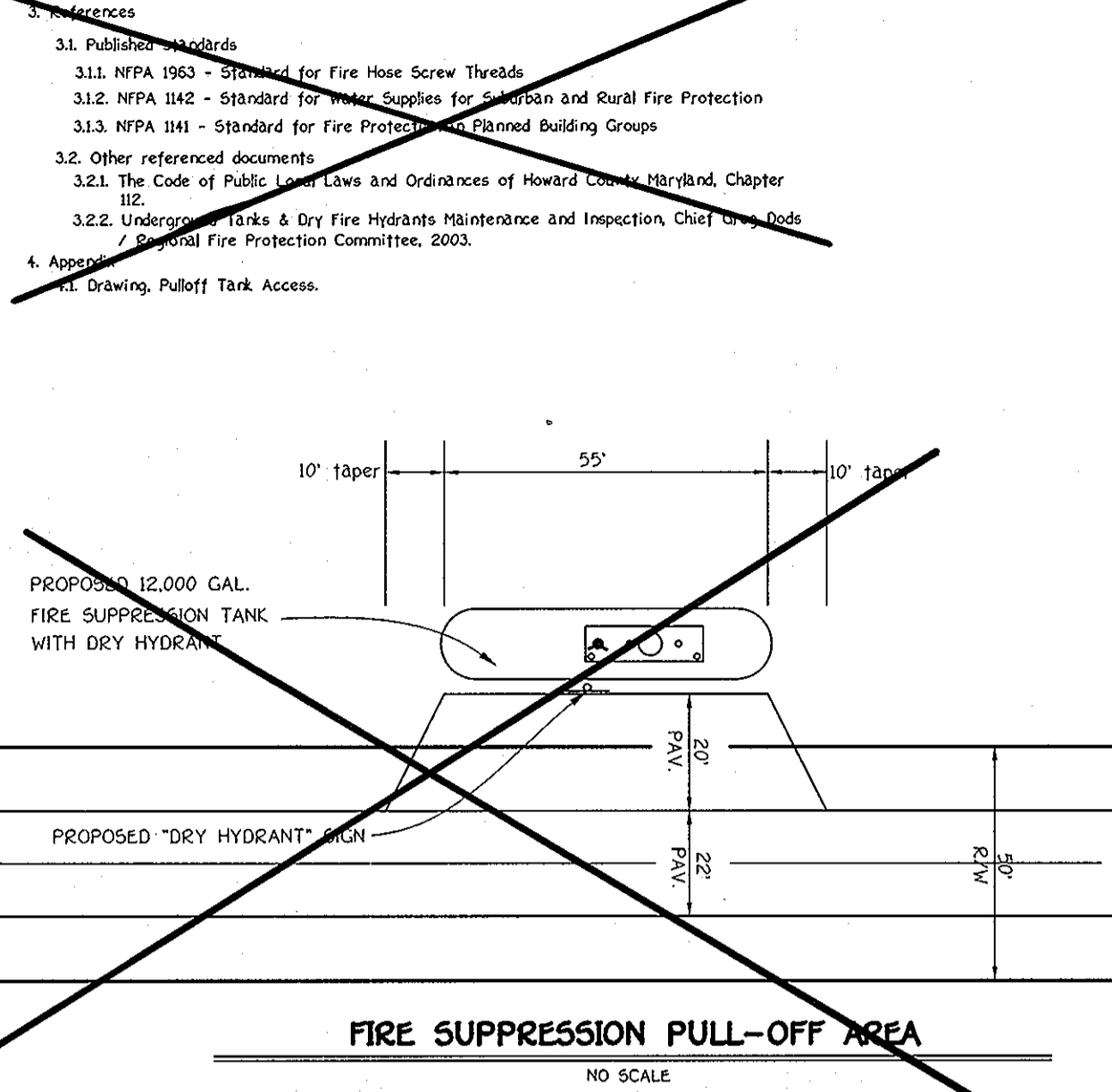
AS BUILT

**SPECIFICATIONS FOR UNDERGROUND WATER STORAGE TANKS FOR FIRE PROTECTION**

1. Introduction
  - 1.1. Scope
 

The construction of underground tanks for fire protection in accordance with Howard County Code, Title 17 and this specification is part of a program designed to improve the level of protection from fires in residential and commercial properties. These specifications, along with associated documents, provide the information necessary for the construction and placement of fire protection tanks used for this purpose.
  - 1.2. Purpose
 

The purpose of this specification is to provide the technical information required for the construction of underground water storage tanks in accordance with Howard County Manual, Title 17 and all data therein referenced.
2. Underground tank
  - 2.1. Materials for a static water supply tank
    - 2.1.1. Tanks shall be constructed of single-wall fiberglass per approved manufacturer's specifications.
    - 2.1.2. The manufacturer shall provide a 30-year warranty, against defects in material for the tank system, to the purchaser of the tank and their heirs, successors, and assigns.
  - 2.2. Features
    - 2.2.1. The tank system shall include a device for holding the tank in position against flotation (deadman). This configuration shall be supplied by the tank manufacturer as part of the tank package. Installation shall be in accordance with the manufacturer's recommendations.
    - 2.2.2. The tank shall include a removable access cover (manhole), at least 48" in diameter, to allow entry for maintenance and inspection of the inside of the tank. An internal ladder shall be securely mounted to the tank. The cover shall be properly secured and locked.
    - 2.2.3. A pad consisting of 4 inch thick concrete shall be placed over the tank, and shall encompass all fittings. Refer to tank drawings for concrete specifications.
  - 2.2.4. Fittings
    - 2.2.4.1. One draft connection shall consist of a 4 inch diameter galvanized or ductile iron pipe. The draft connection shall terminate in a GHI National Hydrant female swivel connector with plug or cap. A PVC draft pipe with an anti-vortex plate shall be installed inside the tank as the manufacturer. The draft connection allows a fire department pumper to connect and remove water from the tank at the specified rate of flow (see flow chart). Draft connection shall be painted "Fire Engine Red."
    - 2.2.4.2. One combination vent/fill indicator shall consist of a 6 inch diameter PVC pipe which allows visual inspection of the tank water level, as well as providing the required amount of venting to the tank.
    - 2.2.4.3. One fill connection shall consist of a 4 inch diameter galvanized or ductile iron pipe, with a hose connection with two, two and one half inch 1/2" connection and one 1/2" connection. The fitting shall be angled downward at 30 degrees to reduce back and fitting strain during filling operations.
    - 2.2.4.4. All pipe joints above the slab shall be flanged.
  - 2.2.5. Any features not included in this specification, as well as any special circumstances such as multiple-tank designs shall be evaluated and approved during design phase and before any construction is started.
  - 2.2.6. The standard tank drawing, Fire Suppression Water Storage, 30,000 Gallon defines the requirements for a typical 30,000 gallon, 10-foot diameter tank installation. Tank sizes other than 30,000 gallons may require additional drawings to be provided to fit the requirements for a particular site. Drawing, Fire Suppression Water Storage, 12,000 Gallon defines the general requirements for construction of 8-foot diameter tanks, and shall be used accordingly. Supplemental drawings shall be reviewed and approved by the Fire Official prior to construction on an as needed basis.
- 2.3. Installation
  - 2.3.1. All tanks shall be installed per the manufacturer's requirements.
  - 2.3.2. Location and orientation of the tank shall be determined during site plan or preliminary subdivision plan layout, as approved by the Fire Official.
  - 2.3.3. The installing contractor shall be responsible for obtaining all necessary permits to work and for scheduling required inspections.
  - 2.3.4. The fittings shall be placed as follows:
    - 2.3.4.1. The draft connection shall be located 8 feet or less from the edge of the parking space or curb line. This allows for one 10 foot section of suction hose to be used to activate. The draft connection opening shall be turned facing directly toward the road surface.
    - 2.3.4.2. Total elevation from the bottom of the suction pipe to the centerline of the draft connection shall not exceed 15 feet zero inches. This dimension shall be minimized as much as possible to reduce head loss, while maintaining correct depth of cover over tank, per manufacturer's specifications.
    - 2.3.4.3. The draft connection shall be fixed at a height of 18 inches above finished grade of the pull off pad, to the centerline of the fitting.
    - 2.3.4.4. The fill connection shall be fixed at a height of 18 inches above finished grade of the pull off pad, to the centerline of the fitting.
  - 2.3.5. Excavation shall be performed per all applicable regulations. The excavation shall be backfilled with a material per manufacturer's specifications. In designs in which the draft pipe is placed on the tank pad, the tank shall have a 3 percent slope toward the draft connection end.
- 2.4. Testing and acceptance
  - 2.4.1. The tank shall be operable with a rate of flow (flow) of 1000 gallons per minute (GPM) minimum using a fire department pumper operating under normal conditions.
  - 2.4.2. A pressure test shall be performed prior to installation of the tank, per manufacturer's specifications.
  - 2.4.3. A pressure test shall be performed after the tank is installed and covered at a maximum pressure of 5 psi (pounds per square inch) not held for a time period as determined by the Fire Official, or per manufacturer's specifications.
  - 2.4.4. An operational test shall be performed when construction is completed, in accordance with the approved procedures. The test shall be conducted by the fire department and the installer. When testing has been completed, the installer shall refill the tank to full capacity as required. Once accepted, further use of the tank shall be limited to firefighting operations and routine testing. The fire department shall be responsible for subsequent refilling to full capacity within 12 hours after using water from the tank.
- 2.4.5. Final approval will be made per notification in writing by the Fire Official.
- 2.5. Accessibility and surrounding features
  - 2.5.1. A pull-off shall be constructed per Appendix 5.1, Drawing 02016. Access to the tank shall be maintained at all times. The requirements for each site will be evaluated and approved by the Fire Official prior to start of work. Details shall be indicated on the site or subdivision plan as applicable.
  - 2.5.2. No obstructions shall impede access to tank fittings. The facility shall remain accessible on a year-round basis.
  - 2.5.3. Landscaping, brush, and trees shall be trimmed away from fittings. Overhanging branches shall be trimmed away at a minimum of 12 feet overhead. Grass and weeds shall be cut as needed.
  - 2.5.4. Protective devices shall be used as applicable to prevent damage to the fittings and to provide safety to operators. These include, but are not limited to the following:
    - a. Bollards
    - b. Guard rails
    - c. Fencing
    - d. Walkways
    - e. Curbs
    - f. Any other barriers/devices as determined by the Fire Official
  - 2.5.5. A reflective sign shall be posted which clearly indicates tank full capacity and identification ID number. An identification number shall be assigned by the county and posted at the site. Signs shall be placed on all major roads indicating the distance and direction to tank with in a 2,500 foot radius.
  - 2.5.6. An approved NO PARKING sign shall be provided and attached to a metal post.
  - 2.5.7. All fittings above grade shall be painted with exterior-grade enamel. Color shall be yellow unless otherwise specified.



NOTE: SEE HOWARD COUNTY STD. DETAILS FOR PAVING SECTION.

**TYPICAL ROADWAY SECTION**  
NO SCALE

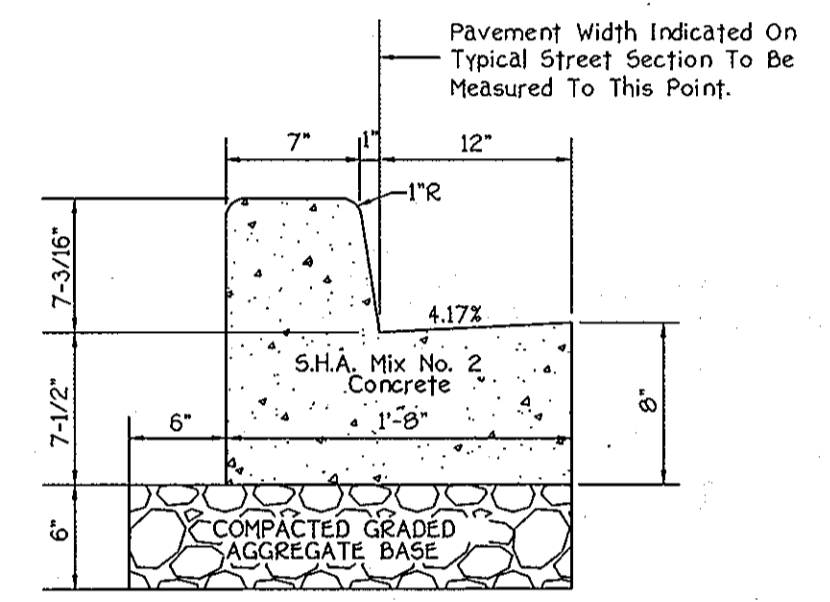
ROADWAY INFORMATION CHART									
ROAD NAME	CLASSIFICATION	DESIGN SPEED	ZONING	A	B	C	STATION LIMITS	PAVING SECTION	
DOUBLE BRIDGES COURT	PUBLIC ACCESS PLACE	25 M.P.H.	RC-DEO	50"	22"	4"	0+00 TO 12+74.69'	P-2	
ROXBURY ROAD	LOCAL ROAD	30 M.P.H.	RC-DEO	50"	24"	4"	28+25.57 TO 33+06.02'	P-3	

SECTION NUMBER	ROAD AND STREET CLASSIFICATION	CALIFORNIA BEARING RATIO (CBR)							
		PAVEMENT-MATERIAL (INCHES)		MIN HMA WITH GAB		HMA WITH CONSTANT GAB			
P-2	PARKING DRIVE AISLES: RESIDENTIAL AND NON-RESIDENTIAL WITH NO MORE THAN 10 HEAVY TRUCKS PER DAY	HMA SUPERPAVE FINAL SURFACE 9.5 MM PG 64-22, LEVEL 1 (ESAL)	1.5	1.5	1.5	1.5	1.5	1.5	
	LOCAL ROADS: ACCESS PLACE, ACCESS STREET	HMA SUPERPAVE INTERMEDIATE SURFACE 9.5 MM PG 64-22, LEVEL 1 (ESAL)	1.0	1.0	1.0	1.0	1.0	1.0	
	CUL-DE-SACS: RESIDENTIAL	HMA SUPERPAVE BASE 19.0 MM PG 64-22, LEVEL 1 (ESAL)	2.0	2.0	2.0	3.5	2.0	2.0	
P-3	PARKING DRIVE AISLES: RESIDENTIAL AND NON-RESIDENTIAL WITH NO MORE THAN 10 HEAVY TRUCKS PER DAY	HMA SUPERPAVE FINAL SURFACE 9.5 MM PG 64-22, LEVEL 1 (ESAL)	1.5	1.5	1.5	1.5	1.5	1.5	
	LOCAL ROADS: ACCESS PLACE, ACCESS STREET	HMA SUPERPAVE INTERMEDIATE SURFACE 9.5 MM PG 64-22, LEVEL 1 (ESAL)	1.0	1.0	1.0	1.0	1.0	1.0	
	CUL-DE-SACS: RESIDENTIAL	HMA SUPERPAVE BASE 19.0 MM PG 64-22, LEVEL 1 (ESAL)	3.0	3.0	3.0	4.5	3.0	2.0	
	MINOR COLLECTORS: RESIDENTIAL	GRADED AGGREGATE BASE (GAB)	10.0	6.0	3.0	6.0	6.0	6.0	

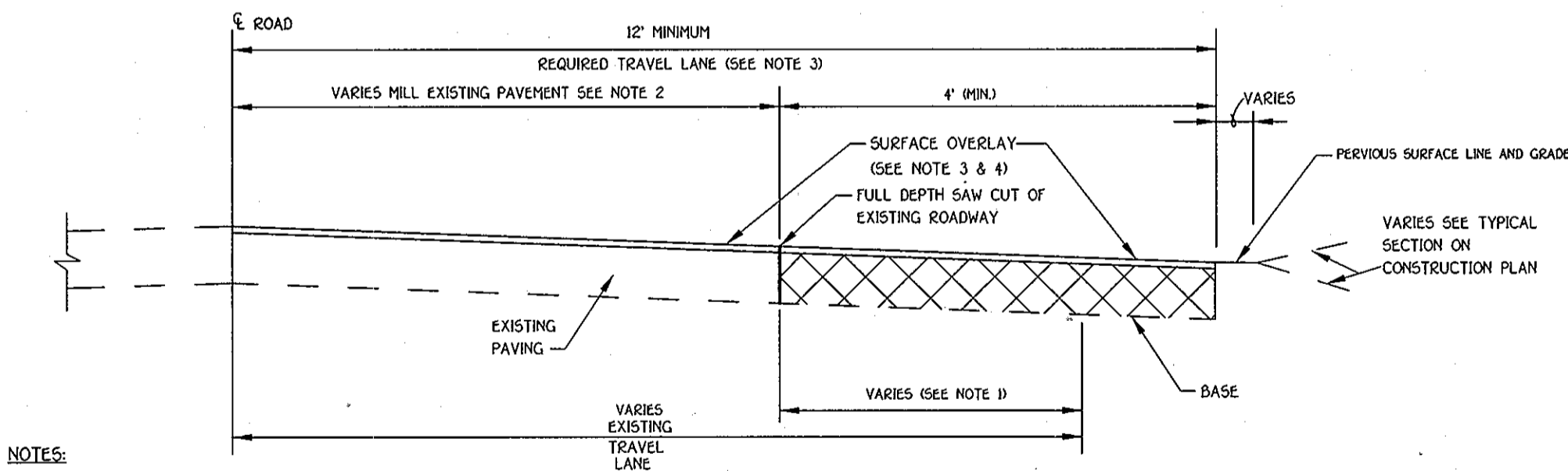
APPROVED: DEPARTMENT OF PUBLIC WORKS  
 Chief, Bureau of Highways  
 DATE: 1-22-09

APPROVED: DEPARTMENT OF PLANNING AND ZONING  
 Chief, Division of Land Development  
 DATE: 2/5/09

Chief, Development Engineering Division  
 DATE: 2/4/09



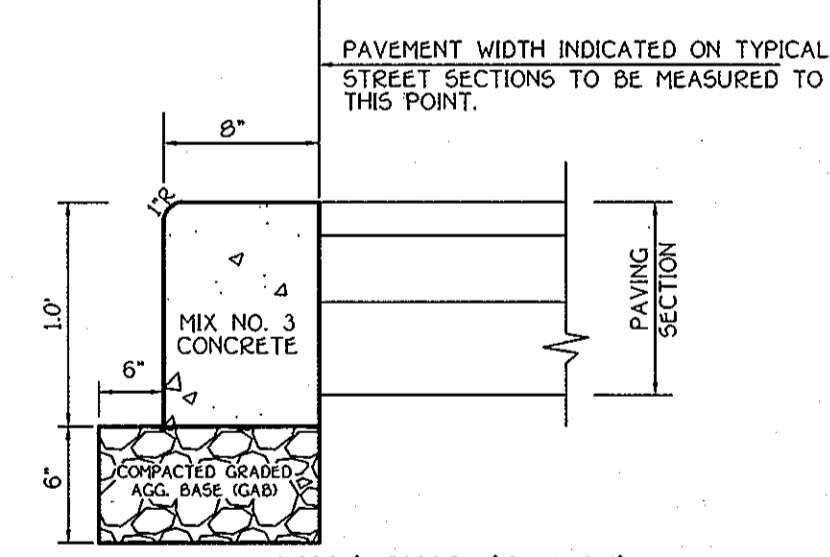
STANDARD 7" COMB. CONC. CURB AND GUTTER  
NO SCALE



- NOTES:
1. WHEN EXISTING TRAVEL LANE IS LESS THAN THE REQUIRED 12' LANE CONTRACTOR SHALL REMOVE A MINIMUM OF 1' FULL DEPTH OF THE EXISTING ROADWAY.
  2. IF CURB AND GUTTER IS INSTALLED, PROVIDE A MINIMUM OF 4" OF WIDENING FROM FACE OF GUTTER PAN.
  3. THE EXISTING PAVEMENT TO BE RESURFACED SHALL BE MILLED AT DEPTH OF 1 1/2" (MINIMUM).
  4. THE RESURFACING SHALL BE PLACED TO THE CENTERLINE OF THE ROADWAY.
  5. THE RESURFACING COURSE TO BE EQUAL TO THE SURFACE COURSE OF THE TYPICAL PAVEMENT SECTION.

EXISTING ROADWAY WIDENING STRIP (R-1.08)  
NO SCALE

NOTE: FLUSH CURB TO BE PLACED ALONG INSIDE RADIUS OF ALL ROAD CURVES WITH LESS THAN 1000' CENTERLINE RADIUS.



FLUSH CURB (R-3.07)  
NO SCALE



**ROADWAY DETAILS**  
**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'  
 ZONED: RC-DEO  
 TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 5 OF 20

FISHER, COLLINS & CARTER, INC.  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 10277 BALDWIN NATIONAL FIVE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 461-2955

No.	Removed Underground Fire Suppression Tank	Date
	Description	Revisions
		4/12/11

**OWNER**  
 FRANCES B. DELVIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7920

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
 (410) 489-7900

ALDO MERIWETHER  
 PROFESSIONAL ENGINEER  
 STATE OF MARYLAND  
 LICENSE NO. 22748  
 EXPIRATION DATE 2-22-09

AS BUILT

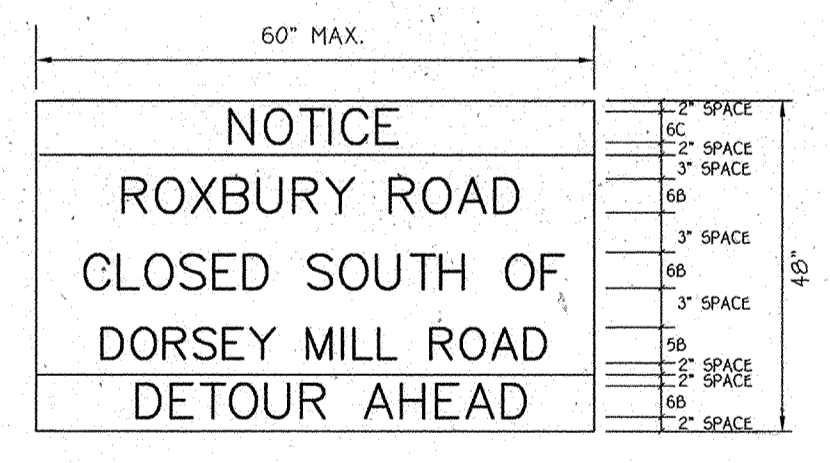
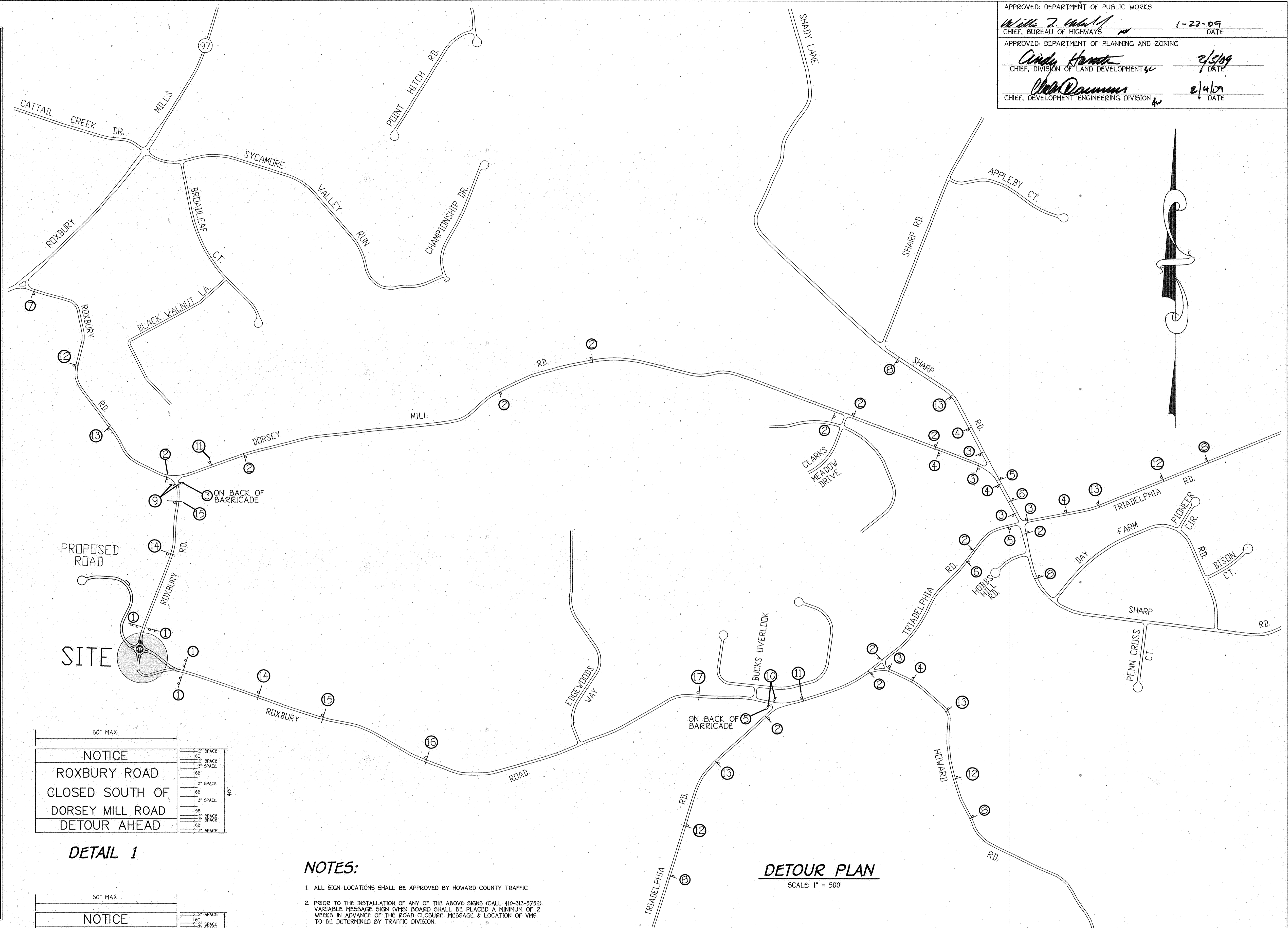
APPROVED: DEPARTMENT OF PUBLIC WORKS  
*William Z. ...* 1-22-09  
 CHIEF, BUREAU OF HIGHWAYS DATE

APPROVED: DEPARTMENT OF PLANNING AND ZONING  
*Carol ...* 2/5/09  
 CHIEF, DIVISION OF LAND DEVELOPMENT DATE

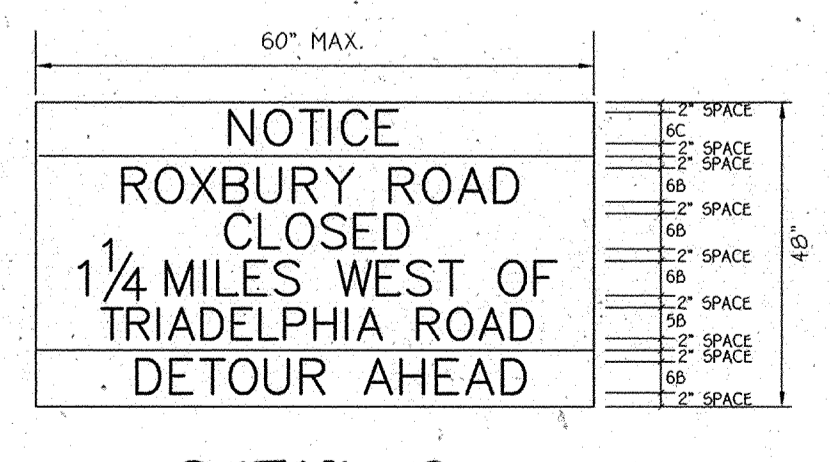
*John ...* 2/4/09  
 CHIEF, DEVELOPMENT ENGINEERING DIVISION DATE

SYMBOL	M.U.T.C.D. DESIGNATION	MESSAGE	SIZE	QUANTITY	COLOR	
					BACKGROUND	CHARACTERS
1*	R11-2	ROAD CLOSED	48"x30"	4	ORANGE	BLACK
2	R4-9 w/ROAD NAME PANEL	ROXBURY ROAD ↑	5" LETTERS 30"x12" 30"x24"	12	WHITE ORANGE	BLACK
3	R4-9R w/ROAD NAME PANEL	ROXBURY ROAD →	5" LETTERS 30"x12" 30"x24"	6	WHITE ORANGE	BLACK
4	R4-9R (MODIFIED ARROW) w/ROAD NAME PANEL	ROXBURY ROAD ↘	5" LETTERS 30"x12" 30"x30"	5	WHITE ORANGE	BLACK
5	R4-9L w/ROAD NAME PANEL	ROXBURY ROAD ←	5" LETTERS 30"x12" 30"x24"	3	WHITE ORANGE	BLACK
6	R4-9L (MODIFIED ARROW) w/ROAD NAME PANEL	ROXBURY ROAD ↙	5" LETTERS 30"x12" 30"x30"	2	WHITE ORANGE	BLACK
7	R11-2A (MODIFIED) SEE DETAIL THIS SHEET	NOTICE ROXBURY ROAD CLOSED SOUTH OF DORSEY MILL ROAD DETOUR AHEAD	60"x48" SEE DETAIL 1	1	YELLOW WHITE ORANGE	BLACK BLACK BLACK
8	R11-2A (MODIFIED) SEE DETAIL THIS SHEET	NOTICE ROXBURY ROAD CLOSED 1 1/4 MILES WEST OF TRIADELPHIA ROAD DETOUR AHEAD	60"x48" SEE DETAIL 2	5	YELLOW WHITE ORANGE	BLACK BLACK BLACK
9*	M4-10L w/R11-3	ROAD CLOSED 1/2 MILE AHEAD LOCAL TRAFFIC ONLY	48"x18" 60"x30"	2	WHITE	BLACK
10*	M4-10R w/R11-3	ROAD CLOSED 1 1/4 MILE AHEAD LOCAL TRAFFIC ONLY	48"x18" 60"x30"	2	WHITE	BLACK
11	M4-0A	END DETOUR	24"x36"	2	ORANGE	BLACK
12	W20-2	DETOUR 1500 FT	48"x48"	4	ORANGE	BLACK
13	W20-2	DETOUR 800 FT	48"x48"	15	ORANGE	BLACK
14	W20-3	ROAD CLOSED 800 FT	48"x48"	2	ORANGE	BLACK
15	W20-3	ROAD CLOSED 1500 FT	48"x48"	2	ORANGE	BLACK
16	W20-3	ROAD CLOSED 1/2 MILE	48"x48"	1	ORANGE	BLACK
17	W20-3	ROAD CLOSED 1 MILE	48"x48"	1	ORANGE	BLACK

\* MOUNT ON TYPE III BARRICADE



DETAIL 1



DETAIL 2

- NOTES:**
- ALL SIGN LOCATIONS SHALL BE APPROVED BY HOWARD COUNTY TRAFFIC
  - PRIOR TO THE INSTALLATION OF ANY OF THE ABOVE SIGNS (CALL 410-313-5752). VARIABLE MESSAGE SIGN (VMS) BOARD SHALL BE PLACED A MINIMUM OF 2 WEEKS IN ADVANCE OF THE ROAD CLOSURE. MESSAGE & LOCATION OF VMS TO BE DETERMINED BY TRAFFIC DIVISION.
  - TYPE III BARRICADES SHALL EXTEND THE FULL WIDTH OF ROADWAY AT CLOSURE POINT AND SHALL HAVE FLASHING WARNING LIGHTS. DRIVEWAY ACCESS TO BE MAINTAINED THROUGHOUT CONSTRUCTION AREA

DETOUR PLAN  
SCALE: 1" = 500'

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 10272 BALTIMORE NATIONAL FREE  
 ELLICOTT CITY, MARYLAND 21114  
 (410) 461-2955

**OWNER**  
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**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
 (410) 489-7900

STATE OF MARYLAND  
*William Z. ...*  
 PROFESSIONAL ENGINEER  
 License No. 20748, Expiration Date 2-22-09

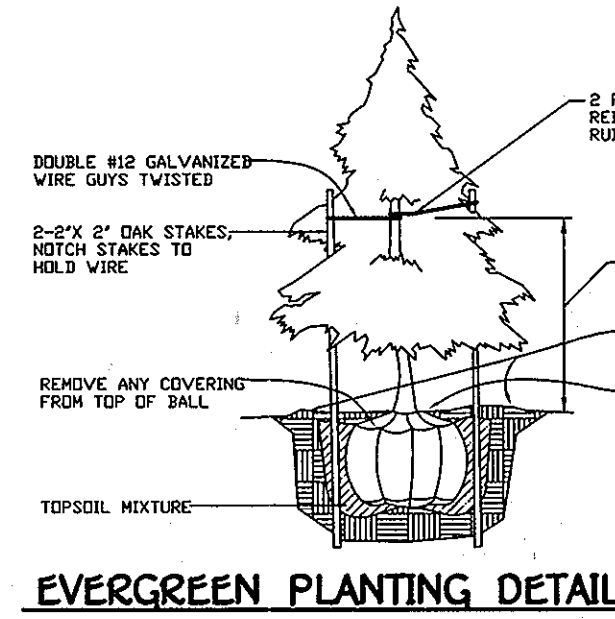
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**DETOUR PLAN  
 MERIWETHER FARM  
 SECTION ONE**  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'  
 ZONED: RC-DEO  
 TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 6 OF 20

"At the time of plant installation, all trees listed and approved on the landscape plan, shall comply with the proper height requirement in accordance with the Howard County Landscape Manual. In addition, no substitutions or relocations of the required plantings may be made without prior review and approval from the Department of Planning and Zoning. Any deviations from the approved Landscape Plan may result in denial or delay in the release of landscape surety until such time as all required materials are planted and/or revisions are made to the road drawing plans."

"The Owner, tenants and/or their agents shall be responsible for maintenance of the required perimeter landscaping. All plant materials shall be maintained in good growing condition and when necessary, replaced with new materials to ensure continued compliance with applicable regulations. All the other required landscaping shall be permanently maintained in good condition, and when necessary, repaired or replaced."

INTERNAL LANDSCAPING S.W.M. PONDS			
ZONE	DESCRIPTION	BMP NO. 3	BMP NO. 2
		ELEVATION	ELEVATION
ZONE 5 FLOORPLAN TERRACE	1' - 4" ELEVATION ABOVE NORMAL POOL ELEVATION - PLANT AREA W/ SWITCH GRASS QUANTITY - N/A SPACING - N/A	500.00 TO 504.00	435.00 TO 439.00
ZONE 3 SHORELINE FRIDGE	0' - 12" ELEVATION ABOVE NORMAL POOL ELEVATION - PLANT AREA W/ BALSAM PINE, WITCHAMAZEL & WINTERBERRY QUANTITY - 15 EACH SPACING - 12" MAX.	499.00 TO 500.00	434.00 TO 435.00
ZONE 2 SHALLOW WATER BENCH	0' - 12" ELEVATION BELOW NORMAL POOL ELEVATION - PLANT AREA W/ WEDGEGRASS QUANTITY - N/A SPACING - N/A	499.00 TO 498.00	434.00 TO 433.00
ZONE 1 DEEPWATER POOL	1' - 3" ELEVATION BELOW NORMAL POOL ELEVATION - PLANT AREA W/ WEDGEGRASS QUANTITY - N/A SPACING - N/A	498.00 TO 494.00	433.00 TO 430.00



PLANT LIST			
SYMBOL	QTY.	BOTANICAL AND COMMON NAME	SIZE
	19	ACER RUBRUM 'OCTOBER GLORY' RED MAPLE	2 1/2 - 3" CAL.
	21	PLATANUS OCCIDENTALIS LONDON PLANETREE	2 1/2 - 3" CAL.
	23	QUERCUS ACUTISSIMA SAWTOOTH OAK	2 1/2 - 3" CAL.
	21	ILEX OPACA AMERICAN HOLLY	5' - 6' HT.
	25	PINUS STROBUS EASTERN WHITE PINE	6' - 8' HT.

Approved: Department of Public Works  
*W. J. M. III* 1-22-09  
 Chief, Bureau of Highways  
 Date

Approved: Department of Planning and Zoning  
*Andy Starnes* 2/5/09  
 Chief, Division of Land Development  
 Date

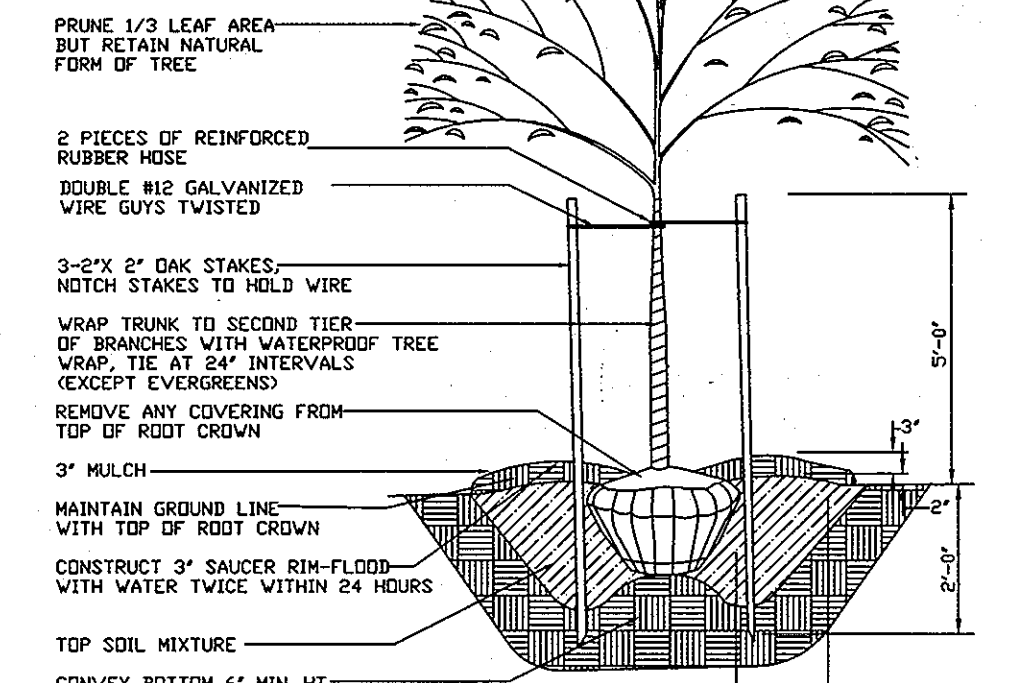
*William* 2/14/09  
 Chief, Development Engineering Division  
 Date

SCHEDULE A - PERIMETER LANDSCAPING						
PERIMETER	CATEGORY	LANDSCAPE TYPE	LINEAR FEET OF ROADWAY FRONTAGE PERIMETER	CREDIT FOR EXISTING VEGETATION (YES, NO, LINEAR FEET) (DESCRIBE BELOW IF NEEDED)	CREDIT FOR WALL, FENCE OR BURN (YES, NO, LINEAR FEET) (DESCRIBE BELOW IF NEEDED)	NUMBER OF PLANTS REQUIRED AND PROVIDED
						SHADE TREES EVERGREEN TREES SHRUBS
P-1	ADJACENT TO ROADWAY	B	25'	YES (079)	NO	1 2 -
P-2	ADJACENT TO PERIMETER	A	597'	YES (250)	NO	6 - -
P-3	ADJACENT TO PERIMETER	A	573'	YES (099)	NO	6 - -
P-4	ADJACENT TO PERIMETER	A	1130'	YES (2077)	NO	15 - -
TOTAL						28 2 -

SCHEDULE D STORMWATER MANAGEMENT AREA LANDSCAPING			
LINEAR FEET OF TYPE 'B' PERIMETER	D-1 : 933'	D-2 : 672'	D-3 : 770'
NUMBER OF TREES REQUIRED:	(933' - 353') = 570'	(672' - 50') = 622'	(770' - 50') = 720'
SHADE TREES	(570' / 50) = 11	(439' / 50) = 9	(770' / 50) = 15
EVERGREEN TREES	(570' / 40) = 14	(439' / 40) = 11	(770' / 40) = 19
CREDIT FOR EXISTING VEGETATION (NO, YES AND %)	YES (363)	YES (233)	NO
CREDIT FOR OTHER LANDSCAPING (NO, YES AND %)	NO	NO	NO
NUMBER OF TREES PROVIDED:	11	9	15
SHADE TREES	14	11	19
EVERGREEN TREES			

This plan has been prepared in accordance with the provision of Section 16.124 of the Howard County Code and Landscape Manual. Financial surety for the required 63 shade and 46 evergreen trees will be posted as part of the Developer's Agreement in the amount of \$25,000.00.

NOTE: CONTRACTOR TO REGRADE, SOD OR HYDRATED AND STRAW MULCH ALL AREAS DISTURBED AS A RESULT OF THEIR WORK. SPRAY WITH WILT-PROOF ACCORDING TO MANUFACTURER'S STANDARDS.



**TRAFFIC CIRCLE PLANTING DETAIL**

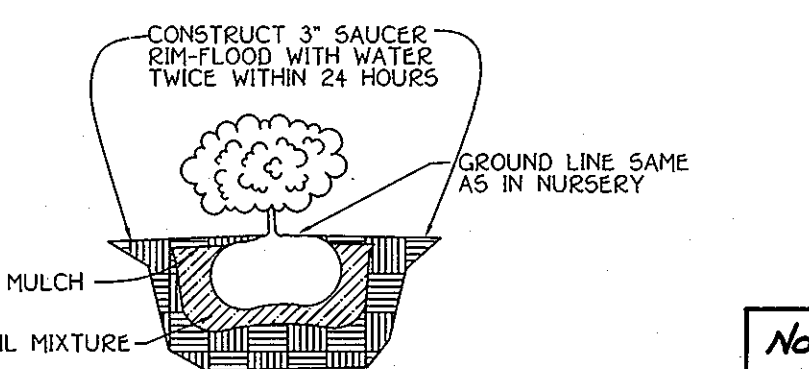
- CONTRACTOR IS RESPONSIBLE FOR ALL TRAFFIC CONTROL MEASURES AND SHALL WORK WITH THE COUNTY ON THE BEST TIME FOR WORK TO TAKE PLACE.
- LANDSCAPE MATERIAL SHALL BE PLANTED BETWEEN APRIL AND MAY OR SEPTEMBER AND OCTOBER.
- ALL EXISTING PLANT MATERIAL SHALL BE REMOVED FROM ISLAND.
- THE OUTER SIX FEET OF THE ISLAND SHALL BE DUG TO THE DEPTH OF THREE INCHES BELOW CURB AND SOIL REMOVED.
- THE DUG OUT AREA SHALL BE COMPACTED USING A SMALL VIBRATOR.
- LANDSCAPE FABRIC SHALL COVER THE COMPACTED AREA AND GO UP THE INNER SOIL EDGE JOINTS IN FABRIC SHALL OVERLAP BY A FOOT.
- 'BRAND RED STONE' 3/4 INCHES IN DIAM SHALL BE ADDED TO THE COMPACTED AREA AND BROUGHT UP TO JUST ABOVE THE TOP OF THE CURB AND INNER CIRCLE OF SOIL.
- THE INNER CIRCLE OF SOIL SHALL BE LOOSED TO A ONE-FOOT DEPTH.
- 3 INCHES THICK OF LEAFGRO OVER TOTAL PLANTING AREA SHALL BE TILLED INTO TOP 6 INCHES.
- CREATE A SLIGHT CROWN IN THE CENTER OF THE ISLAND.
- SHRUBBED HARDWOOD MULCH SHALL BE ADDED TO THE DEPTH OF 2-3 INCHES WITH IT NOT TOUCHING THE BASE OF PLANTS.
- PLANTS SHALL MEET MARYLAND NURSERYMEN SPECIFICATIONS.
- PLANT CENTER TREE (6-8 FT IN HEIGHT AND 1.5 INCH CALIBER) SHALL BE SELECTED FROM THE FOLLOWING LIST:

- |                               |   |                     |                     |
|-------------------------------|---|---------------------|---------------------|
| ACER GINNALIA                 | ACER BUESERIANUM                        | AMUR MAPLE TRIDENT  | MAPLE               |
| AMERLANDBERG LAEVIS 'CUMULUS' | CERCIS CHADWENSIS 'OKLAHOMA'            | CORNELIUS DOGWOOD   | DOGWOOD             |
| CORNUS PHS                    | HAMELIS VIRGINIANA LAGERSTROEMIA        | WITCHAMAZEL         | WITCHAMAZEL         |
| JUNCOA                        | INDICA                                  | CRABAPPLE           | CRABAPPLE           |
| MALUS SPP.                    | GANY. HEAVY DISEASE RESISTANT CULTIVARS | JAPANESE TREE LILAC | JAPANESE TREE LILAC |
| SYRINGA BALSAMIFLORA          | VIBURNUM LANTANA                        | HAWKBIT             | HAWKBIT             |
| VIBURNUM LANTANA              |   | WINTERBERRY         | WINTERBERRY         |
|                               |   | FRAGRANT VIBURNUM   | FRAGRANT VIBURNUM   |
- A RING OF SHORE JUNIPER (UNIPERIS CONFERTA) SHALL BE PLANTED AT A THREE FOOT SPACING ON CENTER AND TWO FEET FROM THE STONE RING 0 PER NINE 50 FT. PLANTS SHALL BE OF A 3 GALLON SIZE.
  - A SELECTION OF FOUR PERENNIALS FROM THE FOLLOWING LIST SHALL BE PLANTED AT 18" ON CENTER 0 PER 2.25 SQ FT IN 4 LARGE GROUP CLUSTERS BETWEEN THE CENTER TREE AND SHORE JUNIPER. DO NOT PLANT PERENNIALS CLOSER THAN 2 FT FROM THE CENTER TREE OR SHORE JUNIPER. PLANTS SHALL BE A 1-GALLON SIZE. (N-NATIVE)
- |                                      |                        |   |
|--------------------------------------|------------------------|---|
| ANEMONE TABERNAMONTANA 'BLUE ICE'    | DWARF BLUESTAR         | N |
| ASCLEPIAS TUBEROSA                   | BUTTERFLYWEED          | N |
| CHRYSOPSIS HARRISII                  | MARSH AND GOLDENROD    | N |
| COREOPSIS VERTICILLATA 'ZAGREB'      | THREADLEAF CORDESPICE  | N |
| ECHINACEA PURPUREA 'KIM'S KNEE HIGH' | DWARF PURPLE CONFLOWER | N |

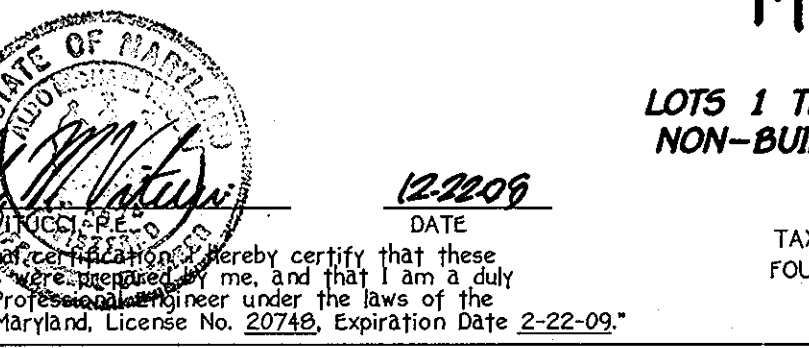
Note: This Plan is for Landscaping Purpose Only.

**TRAFFIC CIRCLE PLANTING DETAIL**

SCALE: 1" = 30'



**SHRUB PLANTING DETAIL**



**LANDSCAPE DEVELOPER'S CERTIFICATE**  
 I/We certify that the landscaping shown on this plan will be done according to the plan, Section 16.124 of the Howard County Code and the Howard County Landscape Manual. I/We further certify that upon completion a certification of landscape installation accompanied by an executed one year guarantee of plant materials will be submitted to the Department of Planning and Zoning.  
*David*  
 Name  
 Date: 12-22-08

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTRAL OFFICE: 10222 BALTIMORE NATIONAL PIKE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 461-2855

**OWNER**  
 FRANCIS & DEVLIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
 (410) 489-7900

**PLAN**  
 SCALE: 1" = 100'

Removed Private Sewer Device For Parcel 'A'	9/18/12	
Removed Underground Fire Suppression Tank f Full-off Area	4/12/11	
No.	Description	Date
	Revisions	

**ALDO M. WILSON, P.E.**  
 PROFESSIONAL ENGINEER  
 STATE OF MARYLAND  
 License No. 20748, Expiration Date 2-22-09

**LANDSCAPE PLAN  
 MERIWETHER FARM  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'**

TAX MAP NO. 21  
 GRID NO. 14, 15, 20 & 21  
 PARCEL NO. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 7 OF 20

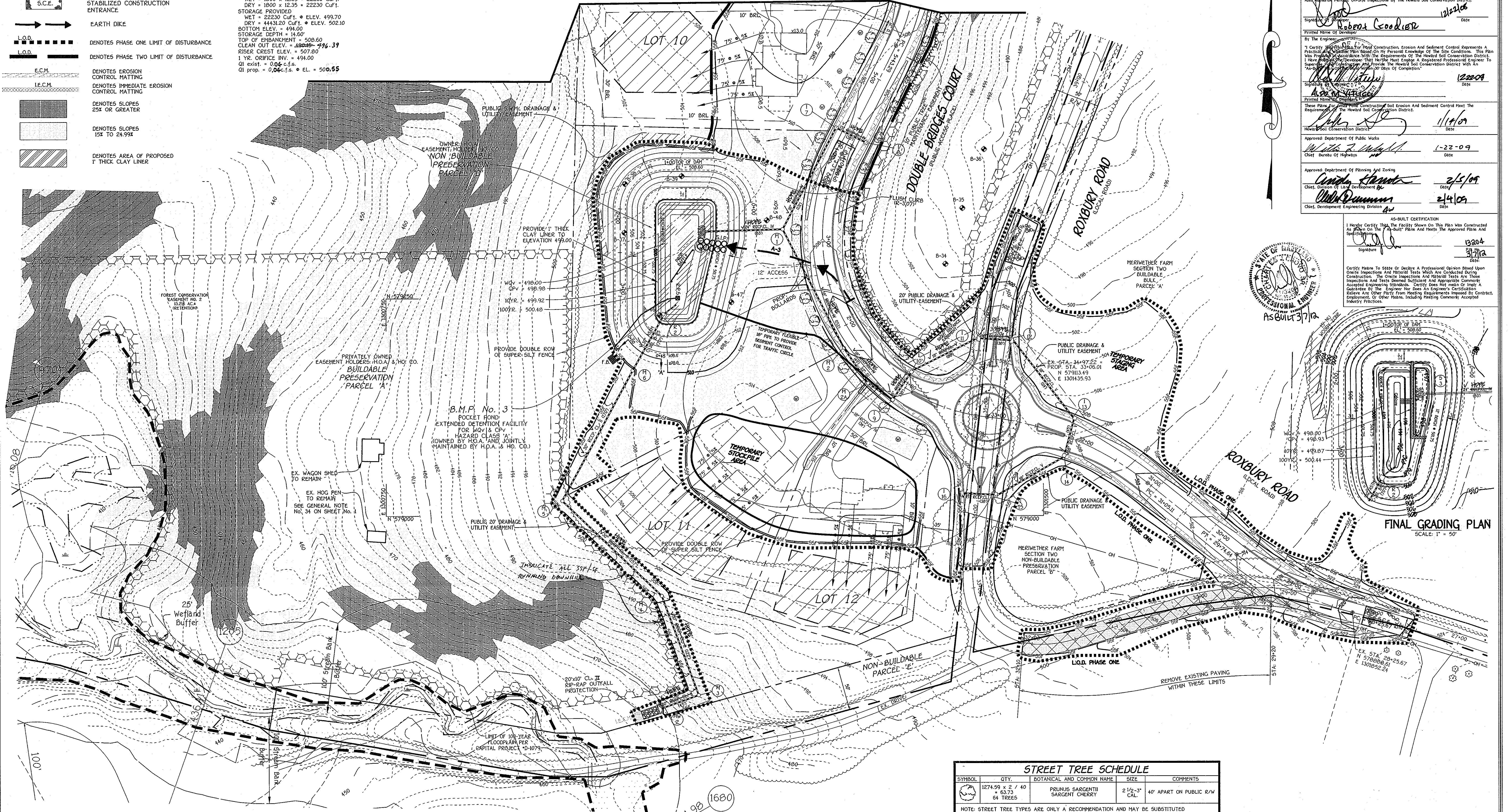
**LEGEND**

- SSF—SSF—SSF— SUPER-SILT FENCE
- SSF—SSF—SSF— DOUBLE ROW SUPER-SILT FENCE
- S.C.E.** STABILIZED CONSTRUCTION ENTRANCE
- EARTH DIKE
- LOD— DENOTES PHASE ONE LIMIT OF DISTURBANCE
- LOD— DENOTES PHASE TWO LIMIT OF DISTURBANCE
- E.C.M. DENOTES EROSION CONTROL MATTING
- I.E.C.M. DENOTES IMMEDIATE EROSION CONTROL MATTING
- 25% OR GREATER DENOTES SLOPES 25% OR GREATER
- 15% TO 24.9% DENOTES SLOPES 15% TO 24.9%
- DENOTES AREA OF PROPOSED 1" THICK CLAY LINER

**TEMPORARY SEDIMENT BASIN No. 2 @ B.M.P. No. 3**

INITIAL D.A. = 12.35 AC.  
FINAL D.A. = 5.17 AC.  
STORAGE REQUIRED  
WET = 1800 x 12.35 = 22230 CUF.  
DRY = 1800 x 12.35 = 22230 CUF.  
STORAGE PROVIDED  
WET = 22230 CUF. @ ELEV. 499.70  
DRY = 4443.20 CUF. @ ELEV. 502.10  
BOTTOM ELEV. = 494.00  
STORAGE DEPTH = 14.60'  
TOP OF EMBANKMENT = 508.60  
CLEAN OUT ELEV. = 488.89-496.39  
RISER CREST ELEV. = 507.80  
1 YR. ORIFICE INV. = 494.00  
QI exist. = 0.04 c.f.s. @ EL. = 500.55  
QI prop. = 0.04 c.f.s. @ EL. = 500.55

MATCH LINE - SEE SHEET 9



By The Developer:  
I/We Certify That All Development And/Or Construction Will Be Done According To These Plans, And That Any Responsible Personnel Involved In The Construction Project Will Have A Certificate Of Attendance At A Department Of The Environment Approved Training Program For The Control Of Sediment And Erosion Before Beginning The Project. I Shall Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion. I Ask Authority Periodic On-Site Inspections By The Howard Soil Conservation District.

Signature of Developer: *Robert Goodier* 12/22/09  
Printed Name of Developer: Robert Goodier

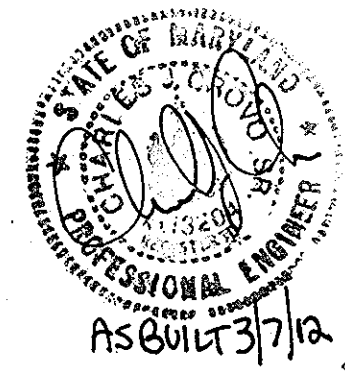
By The Engineer:  
I Certify That These Plans Represent A Final Construction, Erosion And Sediment Control Represents A Practical And Feasible Plan Based On My Personal Knowledge Of The Site Conditions. This Plan Was Prepared In Accordance With The Requirements Of The Howard Soil Conservation District. I Have Made Myself Personally Acquainted With The Site And I, A Registered Professional Engineer, To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion.

Signature of Engineer: *Alfred M. Vetter* 12/22/09  
Printed Name of Engineer: Alfred M. Vetter

Approved Department Of Public Works  
Signature: *William R. ...* 1/14/09  
Chief Bureau Of Highways

Approved Department Of Planning And Zoning  
Signature: *Carolee ...* 2/5/09  
Chief, Division Of Land Development

Signature: *Alfred M. Vetter* 2/4/09  
Chief, Department Engineering Division



**AS-BUILT CERTIFICATION**

I Herby Certify That The Facility Shown On This Plan Was Constructed As Shown On The "As-Built" Plans And Meets The Approved Plans And Specifications.

Signature: *Alfred M. Vetter* 12/22/09  
Date: 12/22/09

Certify Meane To State Or Declare A Professional Opinion Based Upon On-Site Inspections And Historical Tests Which Are Conducted During Construction. The On-Site Inspections And Historical Tests Are Those Inspections And Tests Deemed Sufficient And Appropriate Commonly Accepted Engineering Practices. Certify Does Not Mean Or Imply A Guarantee By The Engineer Nor Does An Engineer's Certification Release Any Other Party From Meeting Requirements Imposed By Contract, Employment, Or Other Means, Including Meeting Commonly Accepted Industry Practices.

**FINAL GRADING PLAN**  
SCALE: 1" = 50'

**STREET TREE SCHEDULE**

SYMBOL	QTY.	BOTANICAL AND COMMON NAME	SIZE	COMMENTS
	1274.99 x 2 / 40 = 6373 64 TREES	PRUNUS SARGENTII SARGENT CHERRY	2 1/2"-3" CAL.	40' APART ON PUBLIC R/W

NOTE: STREET TREE TYPES ARE ONLY A RECOMMENDATION AND MAY BE SUBSTITUTED WITH A COUNTY ACCEPTED EQUIVALENT FROM THE HOWARD COUNTY LANDSCAPE MANUAL. FINANCIAL SURETY FOR THE 64 REQUIRED STREET TREES HAS BEEN POSTED AS PART OF THE DEVELOPER'S AGREEMENT IN THE AMOUNT OF \$10,000.00.

**NOTE:**  
CONTRACTOR SHALL REMOVE ANY AND ALL JUNK, DEBRIS AND TRASH FROM WITHIN THE FLOODPLAIN, BUFFERS AND PRESERVATION PARCELS.

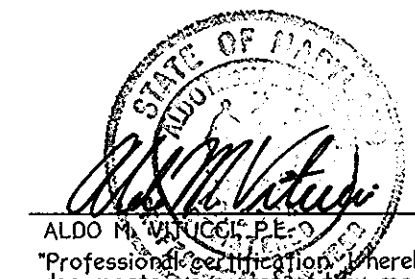
**PLAN**  
SCALE: 1" = 50'

**NOTE:**  
CONTRACTOR SHALL IMBRICATE ALL SUPER SILT FENCE THAT RUNS DOWN HILL.

**FISHER, COLLINS & CARTER, INC.**  
CIVIL, ENGINEERING, CONSULTANTS & LAND SURVEYORS  
CENTENNIAL SQUARE OFFICE PARK - 10272 BALTIMORE NATIONAL PIKE  
ELICOTT CITY, MARYLAND 21042  
(410) 481 - 2855

**OWNER**  
FRANCIS B. DEVLIN  
43 LITCHFIELD ROAD  
LITCHFIELD, CT. 06759  
(410) 489-7900

**DEVELOPER**  
HERITAGE LAND DEVELOPMENT  
15950 NORTH AVENUE, P.O. BOX 482  
LISBON, MARYLAND 21765  
(410) 489-7900



DATE: 12-22-09

**STREET TREE, GRADING & SEDIMENT CONTROL PLAN**  
**MERIWETHER FARM**  
**SECTION ONE**  
LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
AND NON-BUILDABLE PARCEL 'E'

ZONING: RC-DEO  
TAX MAP No. 21  
FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
DATE: DECEMBER 17, 2008  
SHEET 8 OF 20

AS BUILT



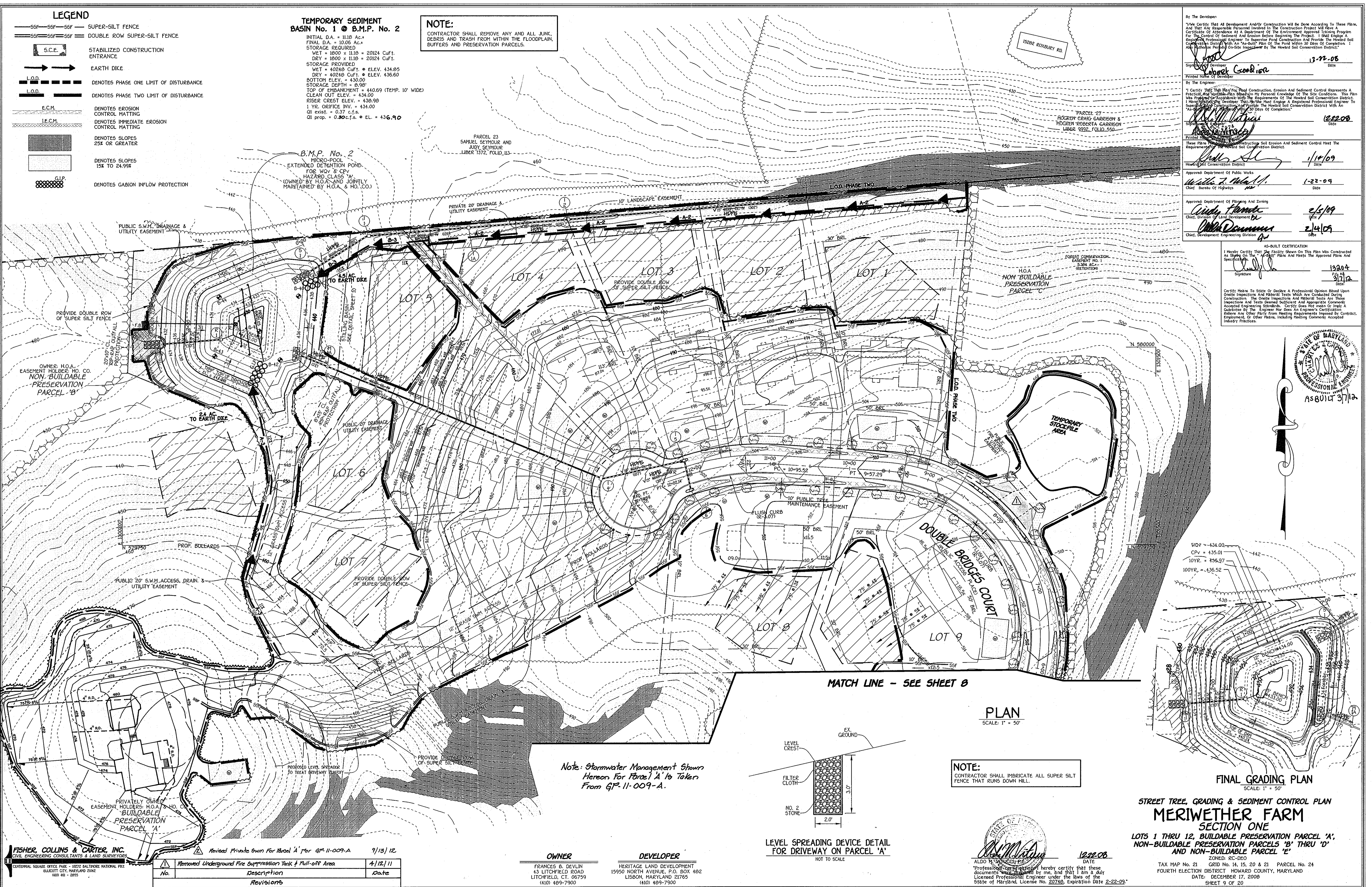
**LEGEND**

- S-S-S— SUPER-SILT FENCE
- ==S-S-S== DOUBLE ROW SUPER-SILT FENCE
- S.C.E. STABILIZED CONSTRUCTION ENTRANCE
- EARTH DIKE
- L.O.D. --- DENOTES PHASE ONE LIMIT OF DISTURBANCE
- L.O.D. --- DENOTES PHASE TWO LIMIT OF DISTURBANCE
- ECM DENOTES EROSION CONTROL MATTING
- IECM DENOTES IMMEDIATE EROSION CONTROL MATTING
- DENOTES SLOPES 25% OR GREATER
- DENOTES SLOPES 15% TO 24.99%
- G.I.P. DENOTES GABION INFLOW PROTECTION

**TEMPORARY SEDIMENT BASIN No. 1 @ B.M.P. No. 2**

INITIAL D.A. = 11.18 AC.  
 FINAL D.A. = 10.06 AC.  
 STORAGE REQUIRED  
 WET = 1000 x 1118 = 2024 CUF.  
 DRY = 1800 x 1118 = 2024 CUF.  
 STORAGE PROVIDED  
 WET = 40248 CUF. @ ELEV. 434.85  
 DRY = 40248 CUF. @ ELEV. 436.60  
 BOTTOM ELEV. = 430.00  
 STORAGE DEPTH = 0.90  
 TOP OF ENHANCEMENT = 440.69 (TEMP. 10' WIDE)  
 CLEAN OUT ELEV. = 434.00  
 RISER CREST ELEV. = 436.98  
 1 YR. ORIFICE INV. = 434.00  
 Q1 exist. = 0.37 c.f.s.  
 Q1 prop. = 0.80 c.f.s. @ EL. = 436.90

**NOTE:**  
 CONTRACTOR SHALL REMOVE ANY AND ALL JUNK, DEBRIS AND TRASH FROM WITHIN THE FLOODPLAIN, BUFFERS AND PRESERVATION PARCELS.



By The Developer:  
 I/We Certify That All Development And/Or Construction Will Be Done According To These Plans, And That Any Responsible Personnel Involved In The Construction Project Will Have A Certificate Of Attendance At A Department Of The Environment Approval Training Program For Control Of Sediment And Erosion Before Beginning The Project. I Shall Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion. I Also Shall Post On-Site Inspection By The Howard Soil Conservation District.

Signature: *Robert Goodwin*  
 Title: Developer  
 Date: 12-22-08

By The Engineer:  
 I Certify That This Final Pond Construction, Erosion And Sediment Control Represents A Practical And Feasible Plan Based On My Personal Knowledge Of The Site Conditions. This Plan Was Prepared In Accordance With The Requirements Of The Howard Soil Conservation District. I Have Informed The Developer That He/She Must Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Within 30 Days Of Completion.

Signature: *[Signature]*  
 Title: [Title]  
 Date: 12-22-08

These Plans Representing Erosion And Sediment Control Meet The Requirements Of The Howard Soil Conservation District.

Signature: *[Signature]*  
 Title: [Title]  
 Date: 1/10/09

Approved Department Of Public Works  
 Chief, Bureau Of Highways

Signature: *[Signature]*  
 Title: [Title]  
 Date: 1-22-09

Approved Department Of Planning And Zoning  
 Chief, Planning And Zoning

Signature: *[Signature]*  
 Title: [Title]  
 Date: 2/4/09

AS-BUILT CERTIFICATION  
 I Herby Certify That The Facility Shown On This Plan Was Constructed As Shown On The "As-Built" Plans And Meets The Approved Plans And Specifications.

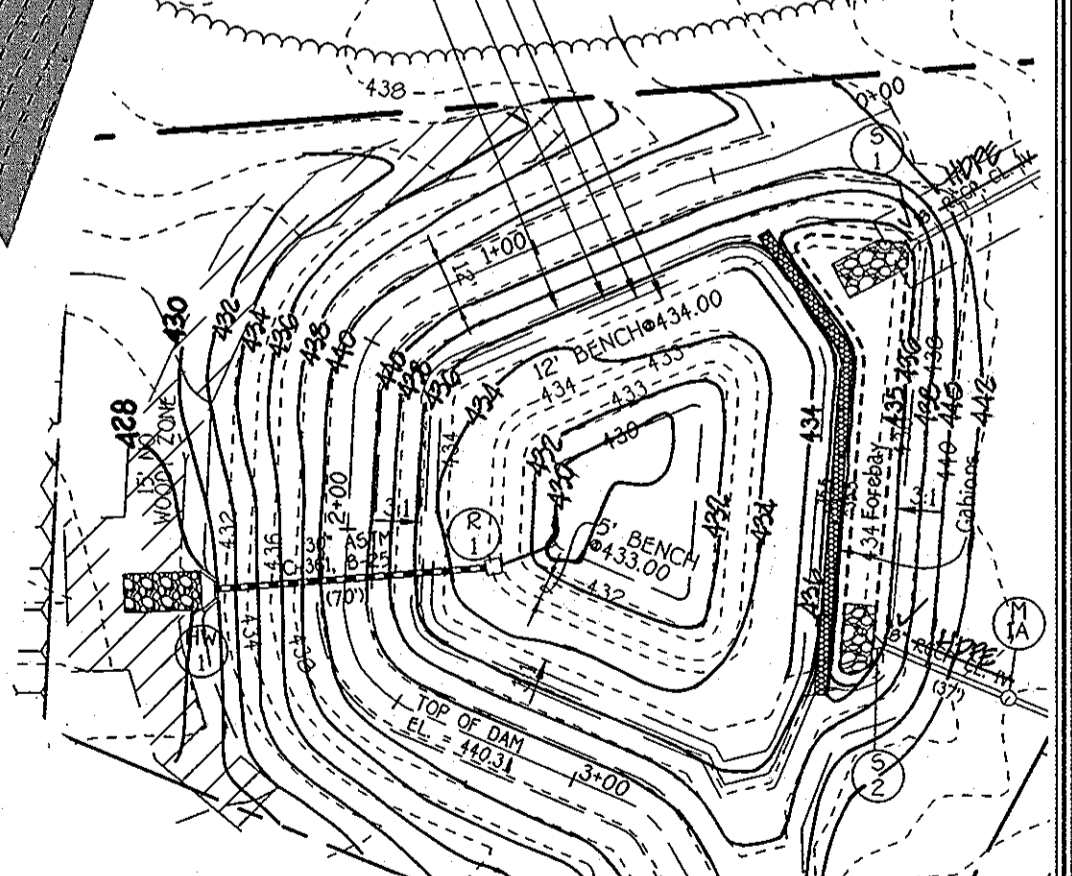
Signature: *[Signature]*  
 Title: [Title]  
 Date: 3/11/09

I Certify Means To State Or Declare A Professional Opinion Based Upon Onsite Inspections And Material Tests Which Are Conducted During Construction. The Onsite Inspections And Material Tests Are Those Inspections And Tests Deemed Sufficient And Appropriate Commonly Accepted Engineering Standards. Certify Does Not Mean Or Imply A Guarantee By The Engineer Nor Does An Engineer's Certification Foresee Any Other Party From Meeting Requirements Imposed by Contract, Employment, Or Other Means, Including Meeting Commonly Accepted Industry Practices.

Signature: *[Signature]*  
 Title: [Title]  
 Date: 3/11/09



10YR = 434.00  
 10YR = 435.01  
 10YR = 436.97  
 100YR = 436.52



**FINAL GRADING PLAN**  
 SCALE: 1" = 50'

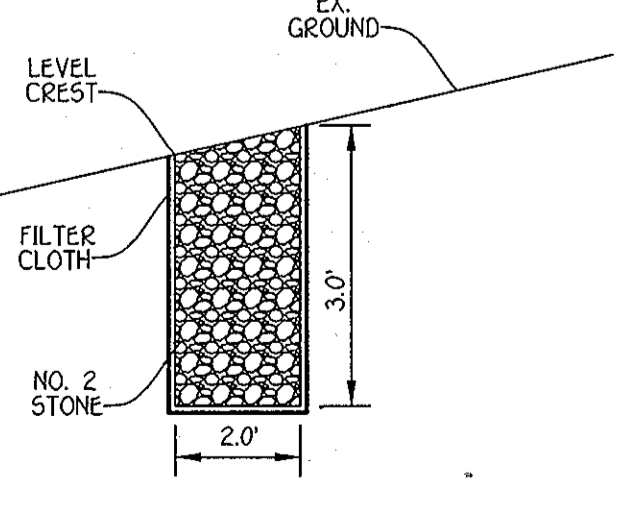
**STREET TREE, GRADING & SEDIMENT CONTROL PLAN**  
**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D',  
 AND NON-BUILDABLE PARCEL 'E'

TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 9 OF 20

MATCH LINE - SEE SHEET 8

**PLAN**  
 SCALE: 1" = 50'

Note: Stormwater Management Shown Hereon For Parcel 'A' Is Taken From GP-11-009-A.



**NOTE:**  
 CONTRACTOR SHALL IMBRICATE ALL SUPER SILT FENCE THAT RUNS DOWN HILL.



12-22-08

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 10272 BALTIMORE NATIONAL FREE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 461-2855

No.	Description	Date
1	Removed Underground Fire Suppression Tank & Full-off Area	4/12/11
2	Revised Private Sewer For Parcel 'A' Per GP-11-009-A	9/13/12

**OWNER**  
 FRANCIS & DEVLIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
 (410) 489-7900

AS BUILT

**STRUCTURE SCHEDULE**

STRUCTURE NO.	TOP ELEVATION	INV. IN	INV. OUT	LOCATION	STATION	OFFSET	TYPE	REMARKS
I-1	450.49.20	445.45.0	443.45.24	N 5801864E 4070 E 1300510.59.1	-----	-----	D' INLET	D - 4.10 (PRIVATE)
I-1A	462.00.461.82	456.26.07	455.95.70	N 5801464E 51.31 E 1300635.46.40	-----	-----	D' INLET	D - 4.10 (PRIVATE)
I-2	465.50.460.75	461.23.11	460.94.01.03	N 5801864E 41.05 E 1300761.54.73	-----	-----	D' INLET	D - 4.10 (PRIVATE)
I-3	467.63.70	464.25.02	464.25.02	N 5801864E 41.05 E 1301062E 1.70	-----	-----	YARD INLET	D - 4.14 (PRIVATE)
I-4	497.43.496.23	491.60.05	489.66.74	DOUBLE BRIDGES COURT	L.P. 1+78.90.07	-----	A-5 INLET	D - 4.01
I-5	500.00.494.03	495.37.17	494.12.49.4.93	DOUBLE BRIDGES COURT	12+07.73.027	494.12.9.1	K' INLET	D - 4.12 W/D - 4.13
I-6	500.00.497.03	495.75.00	495.75.00	DOUBLE BRIDGES COURT	12+04.98.0.134	494.12.9.1	K' INLET	D - 4.12 W/D - 4.13
I-7	504.75.4	500.49.12	499.24.2	DOUBLE BRIDGES COURT	4+28.57.40	19.1	K' INLET	D - 4.12 W/D - 4.13
I-8	504.75.03	500.49.12	500.49.12	DOUBLE BRIDGES COURT	4+28.57.40	19.1	K' INLET	D - 4.12 W/D - 4.13
I-9	507.00.0.03	499.72.00	499.66.19	DOUBLE BRIDGES COURT	1+04.00.2.50	19.1	K' INLET	D - 4.12 W/D - 4.13
I-10	507.00.0.03	500.00.497.03	499.44.80	DOUBLE BRIDGES COURT	1+04.00.2.50	19.1	K' INLET	D - 4.12 W/D - 4.13
I-11	504.82	500.54.42	500.44.70	ROXBURY ROAD	35+07.64.70	16.6	A-5 INLET	D - 4.12 W/D - 4.13
I-12	504.82.7	500.54.77	500.73.50	ROXBURY ROAD	35+07.64.70	16.6	A-5 INLET	D - 4.12 W/D - 4.13
I-13	507.00.0.03	502.00.100	501.93.73	ROXBURY ROAD RELOCATION	32+44.00.9.97	24.4	K' INLET	D - 4.12 W/D - 4.13
I-14	507.00.0.03	502.36.0	502.26.13	ROXBURY ROAD RELOCATION	32+06.53.8.75	24.4	K' INLET	D - 4.12 W/D - 4.13
I-15	506.65.9	503.09.3	502.84.3	ROXBURY ROAD	34+09.42.07	17.6	A-5 INLET	D - 4.12 W/D - 4.13
I-16	506.65.70	503.20.3	503.20.3	ROXBURY ROAD	34+07.46.20	17.6	A-5 INLET	D - 4.12 W/D - 4.13
M-1	500.40.07	490.25.490.63.70	497.75.70	N 579329.42.70 E 1301202.05.03	-----	-----	STD. MANHOLE	G - 5.12
M-1A	444.00.30.03	438.55.22	435.52.34	N 579974.37.25 E 1300461.95.97.23	-----	-----	STD. MANHOLE	G - 5.12
M-2	500.00.30.03	499.00.0.03	498.67.0.03	DOUBLE BRIDGES COURT	1+74.59.0.03	46.1	STD. MANHOLE	G - 5.12
M-2A	509.25.74	499.45.70	499.35.27	DOUBLE BRIDGES COURT	-----	-----	STD. MANHOLE	G - 5.12
M-3	476.50.44	470.21.0.03	468.71.24	N 578794.23.76.96 E 1301106.11.05.00	-----	-----	STD. MANHOLE	G - 5.12
M-4	494.00.3.70	486.40.0.03	485.40.0	N 578900.16.0.03 E 1301064.32.42.94	-----	-----	STD. MANHOLE	G - 5.12
M-5	500.40.73	491.50.13	491.25.0.03	N 579047.52.34.60 E 1300960.96.11.27	-----	-----	STD. MANHOLE	G - 5.12
M-6	500.40.73	491.25.0.03	492.00.75	N 579167.30.0.77 E 1301094.40.0.03	-----	-----	STD. MANHOLE	G - 5.12
S-1	436.50.70	436.00.4.00	-----	N 580094.45.9.19 E 1300438.03.1.54	-----	-----	CONC. END SECTION	D - 5.51 (PRIVATE)
S-2	436.50.70	435.00.0.03	-----	N 579989.96.8 E 1300423.93.0.07	-----	-----	CONC. END SECTION	D - 5.51
S-3	499.40.0.03	497.00.0.03	-----	N 579326.76.7.02 E 1301022.05.10.02	-----	-----	CONC. END SECTION	D - 5.51
HW-1	434.00.0.03	427.00.0.03	-----	N 580004.58.4.3 E 1300251.09.3	-----	-----	TYPE 'A' HEADWALL	D - 5.11
HW-2	469.05.7	465.05.7	-----	N 578778.94.1.02 E 1301064.12.10.99	-----	-----	TYPE 'A' HEADWALL	D - 5.11
R-1	438.90.2	436.00.0.03	429.65.70.1	N 580010.25.30 E 1300323.74.1.1	-----	-----	CONCRETE RISER	SEE SHEET 14
R-2	507.00.10	494.00.3.03	493.40.77	N 579230.6.2.44 E 1301066.51.7.77	-----	-----	CONCRETE RISER	SEE SHEET 15

\* DENOTES THROAT ELEVATION  
\*\* DENOTES TOP OF GRATE ELEVATION

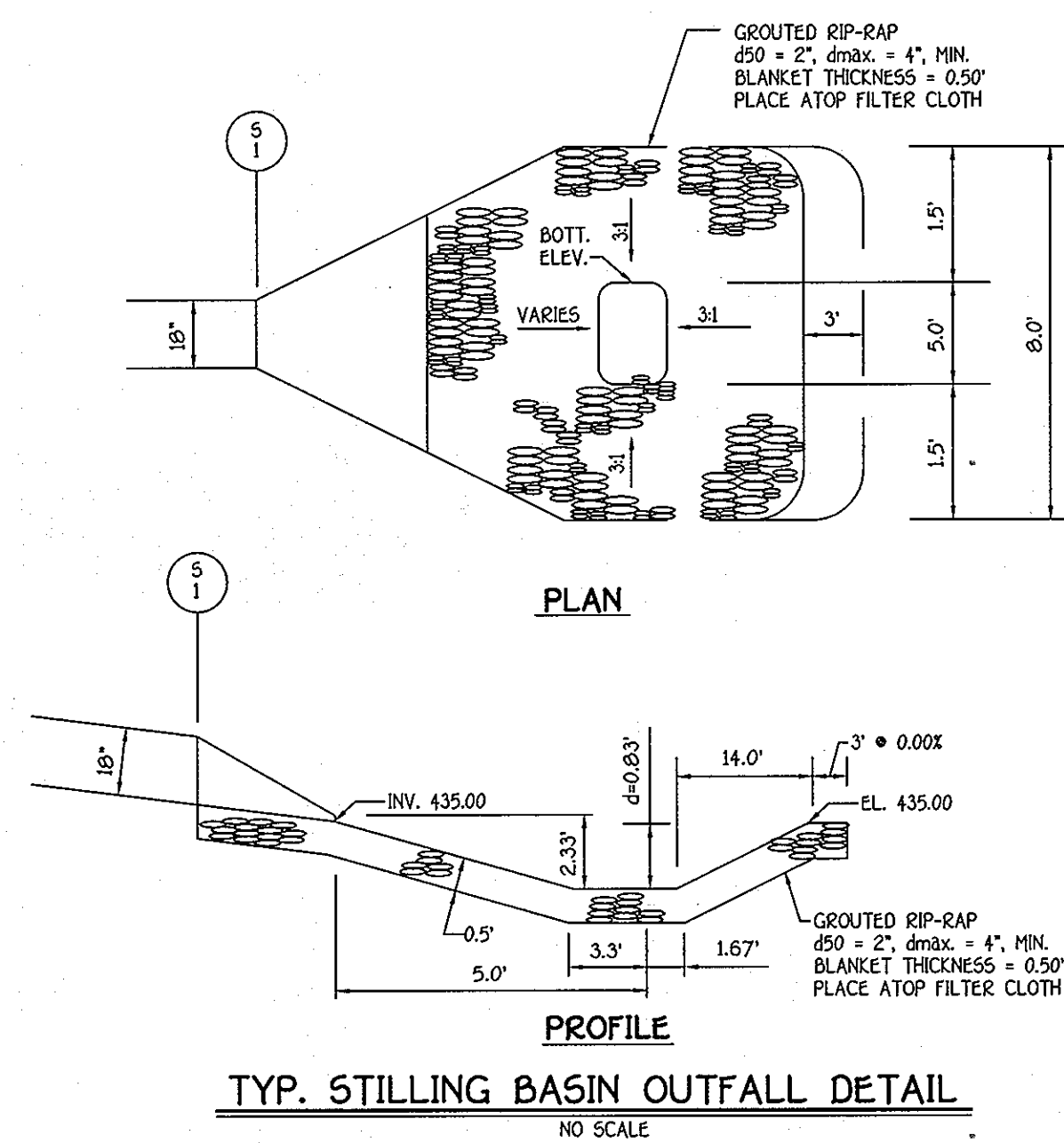
**PUBLIC PIPE SCHEDULE**

SIZE	CLASS	LENGTH
18"	HOPE ROCCP CL. IV	70 L.F. ✓
18"	ROCCP CL. VV	33 L.F. ✓
18"	ROCCP CL. IV	279 L.F. ✓
18"	HOPE	104 L.F. ✓
24"	ROCCP CL. VV	11 L.F. ✓
24"	ROCCP CL. IV	83 L.F. ✓
30"	HOPE ROCCP CL. II	486 L.F. ✓
30"	ASTM C-361 B-25	70 L.F. ✓
30"	ASTM C-361 C-22	80 L.F. ✓

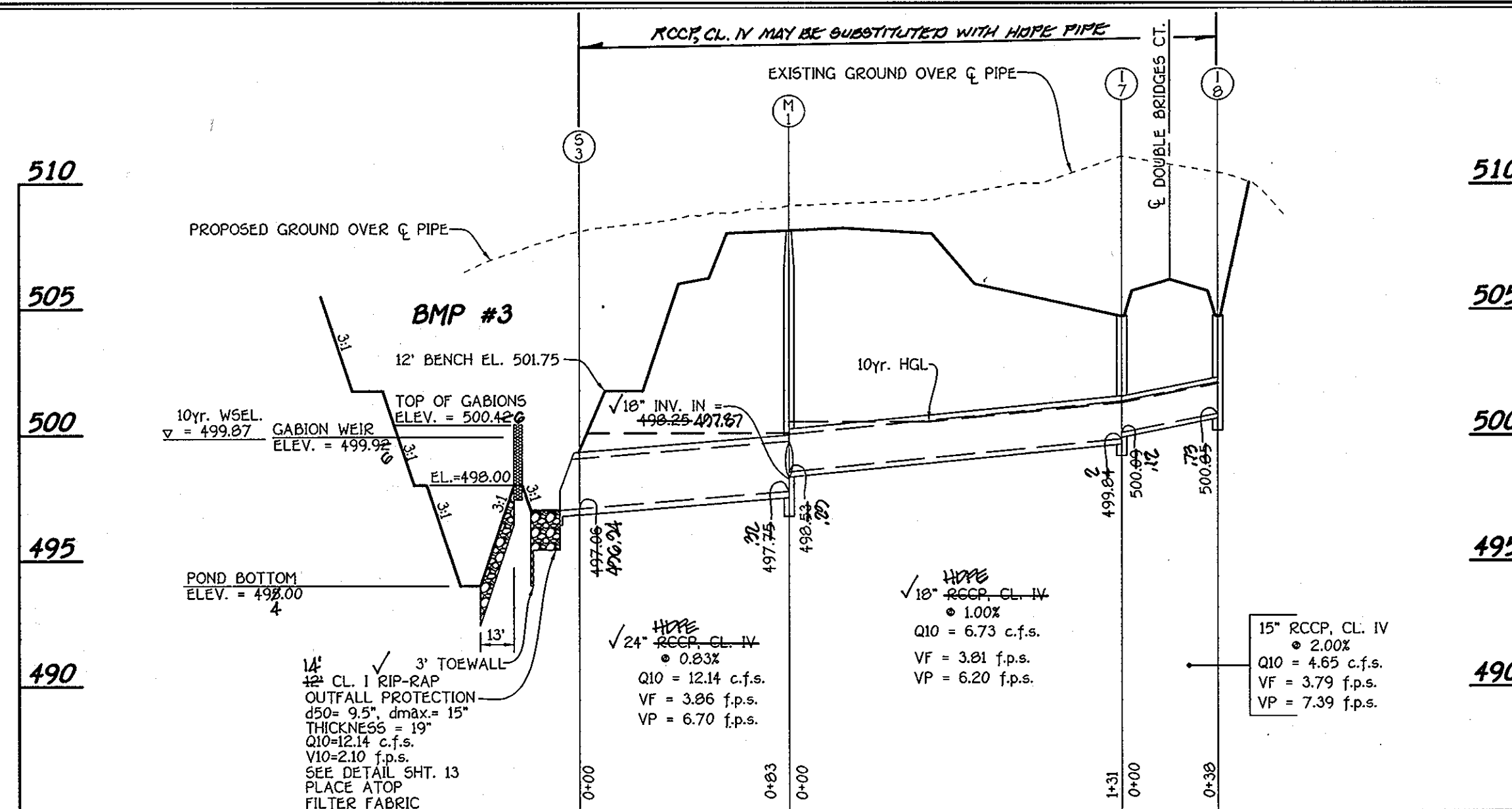
**PRIVATE PIPE SCHEDULE**

SIZE	CLASS	LENGTH
15"	ROCCP CL. IV	302 L.F. ✓
18"	ROCCP CL. IV	333 L.F. ✓

NOTE: ROCCP CL. II OR ROCCP CL. IV MAY BE SUBSTITUTED WITH HOPE PIPE.

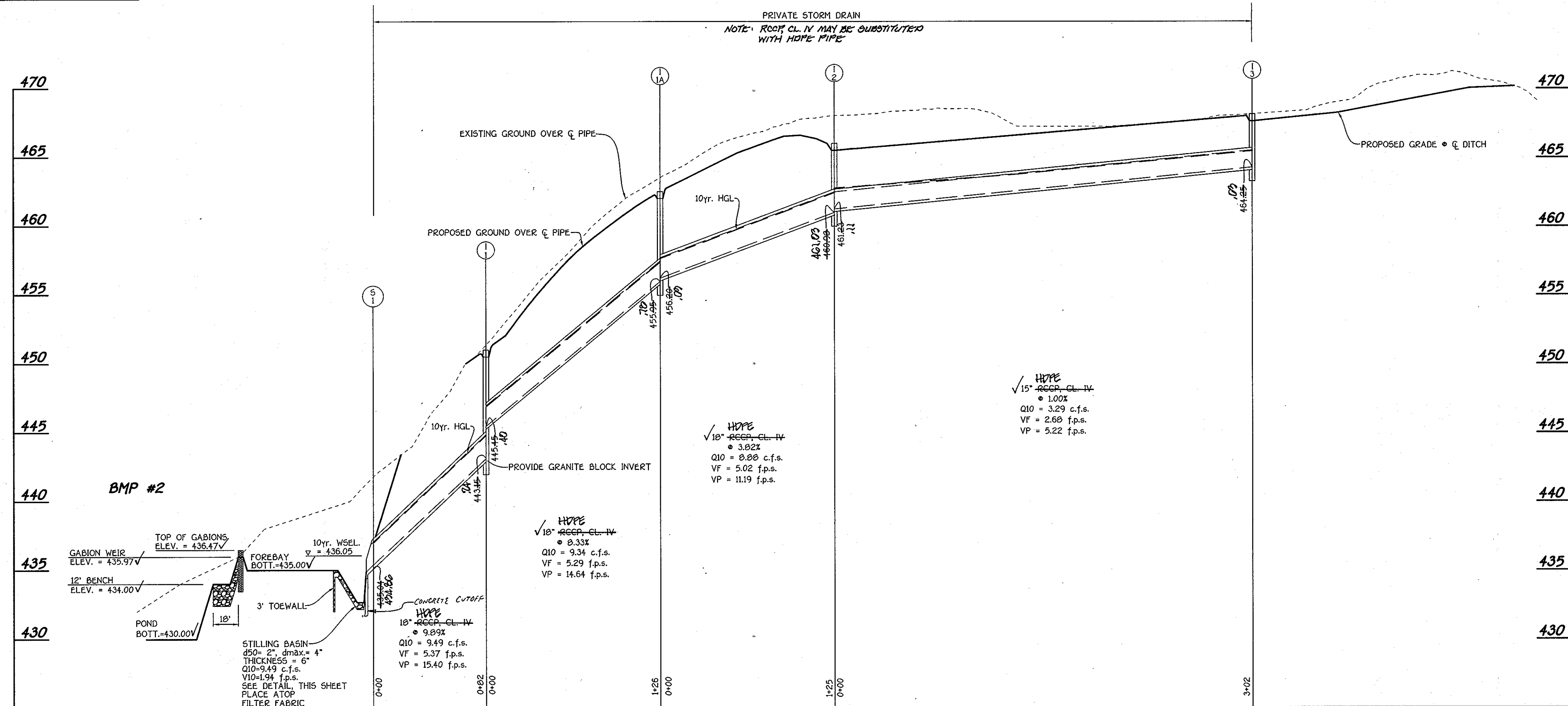


TYP. STILLING BASIN OUTFALL DETAIL  
NO SCALE



**PROFILE**

SCALE: HORIZ. 1" = 50'  
VERT. 1" = 5'



**PROFILE**

SCALE: HORIZ. 1" = 50'  
VERT. 1" = 5'

APPROVED: DEPARTMENT OF PUBLIC WORKS  
 [Signature] 1-22-09 DATE  
 CHIEF, BUREAU OF HIGHWAYS  
 APPROVED: DEPARTMENT OF PLANNING AND ZONING  
 [Signature] 2/10/09 DATE  
 CHIEF, DIVISION OF LAND DEVELOPMENT  
 APPROVED: [Signature] 2/4/09 DATE  
 CHIEF, DEVELOPMENT ENGINEERING DIVISION

**DRIVEWAY CULVERT DATA**

LOT	STA. AND SLOPE (X)	ROAD NAME	PIPE SIZE	REMARKS
1	9+11 @ 4.00%	DOUBLE BRIDGES CT.	12"	* Q10=7.2 c.f.s.
2	10+20 @ 4.00%	DOUBLE BRIDGES CT.	12"	* Q10=7.2 c.f.s.
3	11+54 @ 4.00%	DOUBLE BRIDGES CT.	12"	* Q10=7.2 c.f.s.
4	DOWNGRADE CUL-DE-SAC	DOUBLE BRIDGES CT.	NO PIPE	NO PIPE
5	N/A SUMP	DOUBLE BRIDGES CT.	NO PIPE	NO PIPE
6	N/A SUMP	DOUBLE BRIDGES CT.	NO PIPE	NO PIPE
7	DOWNGRADE CUL-DE-SAC	DOUBLE BRIDGES CT.	NO PIPE	NO PIPE
PAR. A	DOWNGRADE CUL-DE-SAC	DOUBLE BRIDGES CT.	NO PIPE	NO PIPE
8	10+25 @ 4.00%	DOUBLE BRIDGES CT.	12"	* Q10=7.2 c.f.s.
9	0+74 @ 3.80%	DOUBLE BRIDGES CT.	12"	* Q10=7.05 c.f.s.
10	5+59 @ 3.12%	DOUBLE BRIDGES CT.	12"	* Q10=6.50 c.f.s.
PAR. D	N/A HIGH POINT	DOUBLE BRIDGES CT.	NO PIPE	NO PIPE
11	1+45 @ 3.40%	DOUBLE BRIDGES CT.	12"	* Q10=6.70 c.f.s.
12	1+23 @ 3.43%	DOUBLE BRIDGES CT.	12"	* Q10=6.72 c.f.s.

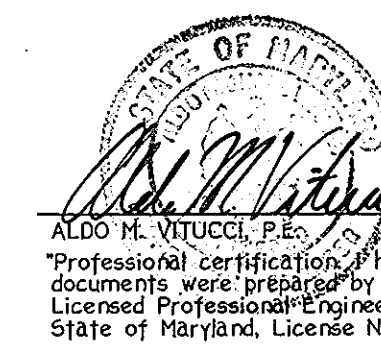
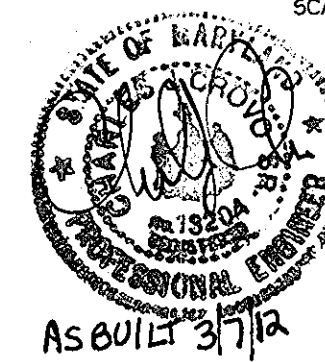
\* - DENOTES MAXIMUM PIPE CAPACITY BASED ON SLOPE

FISHER, COLLINS & CARTER, INC.  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 12700 NATIONAL SQUARE OFFICE PARK - 10772 BALTIMORE NATIONAL FEE  
 BELLCOTT CITY, MARYLAND 21042  
 (410) 481-2255

No.	Description	Date
1	Added Storm Drain Pipe Substitution Note	12-1-10

**OWNER**  
 FRANCES B. DEVLIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
 (410) 489-7900



DATE: 12/22/09

Professional certification is hereby certified that these documents were prepared by me, and that I am a duly Licensed Professional Engineer under the laws of the State of Maryland, License No. 22748, Expiration Date 2-22-09.

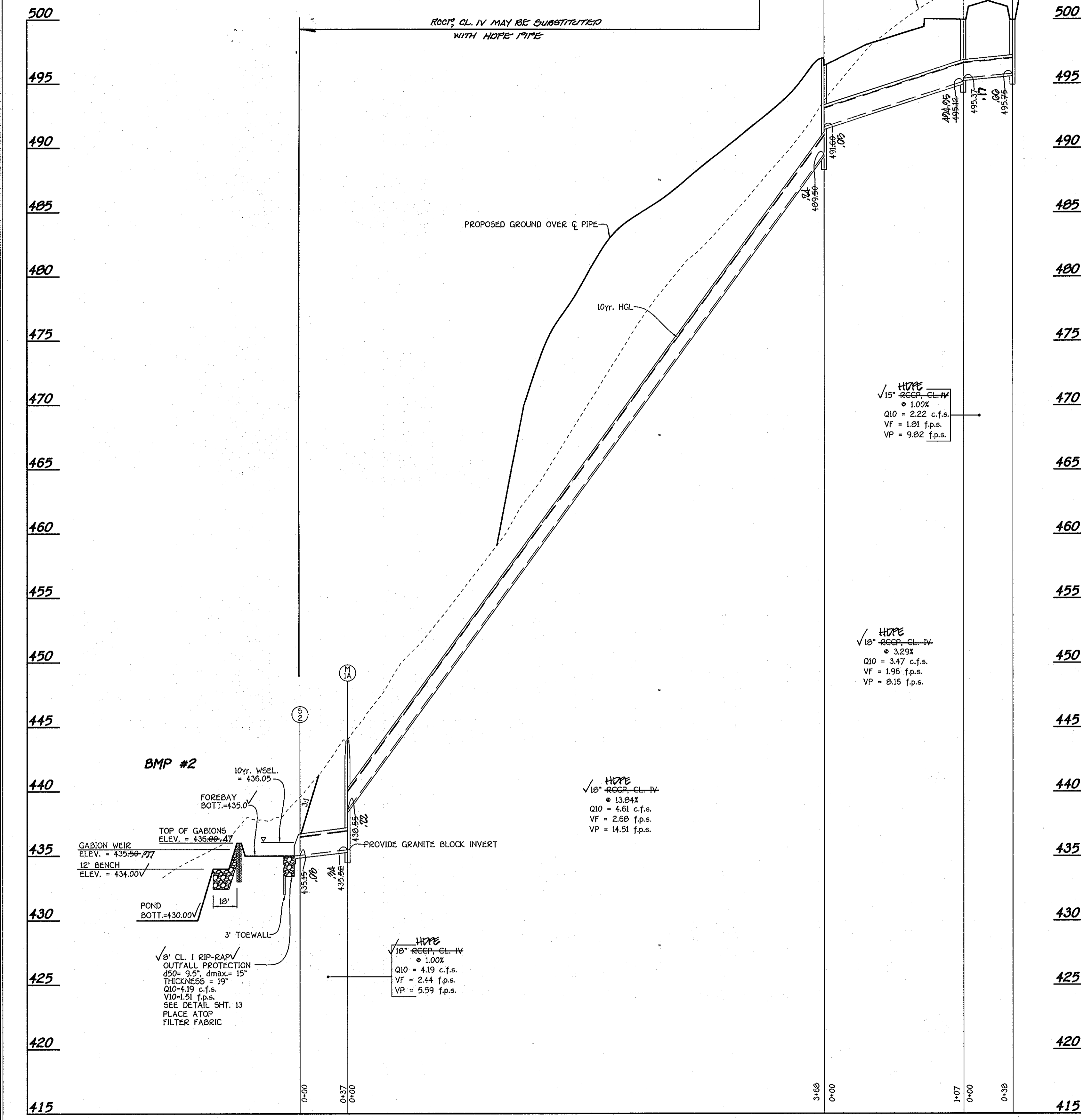
**STORM DRAIN PROFILES**  
**MERIWETHER FARM**  
**SECTION ONE**  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'  
 ZONED: RC-DEO  
 TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 10 OF 20

AS BUILT

APPROVED: DEPARTMENT OF PUBLIC WORKS  
 Mike J. DeWalt, 1-22-09  
 CHIEF, BUREAU OF HIGHWAYS DATE

APPROVED: DEPARTMENT OF PLANNING AND ZONING  
 Cindy Hamer, 2/18/09  
 CHIEF, DIVISION OF LAND DEVELOPMENT DATE

APPROVED: DEPARTMENT OF PLANNING AND ZONING  
 John Deane, 2/14/09  
 CHIEF, DEVELOPMENT ENGINEERING DIVISION DATE

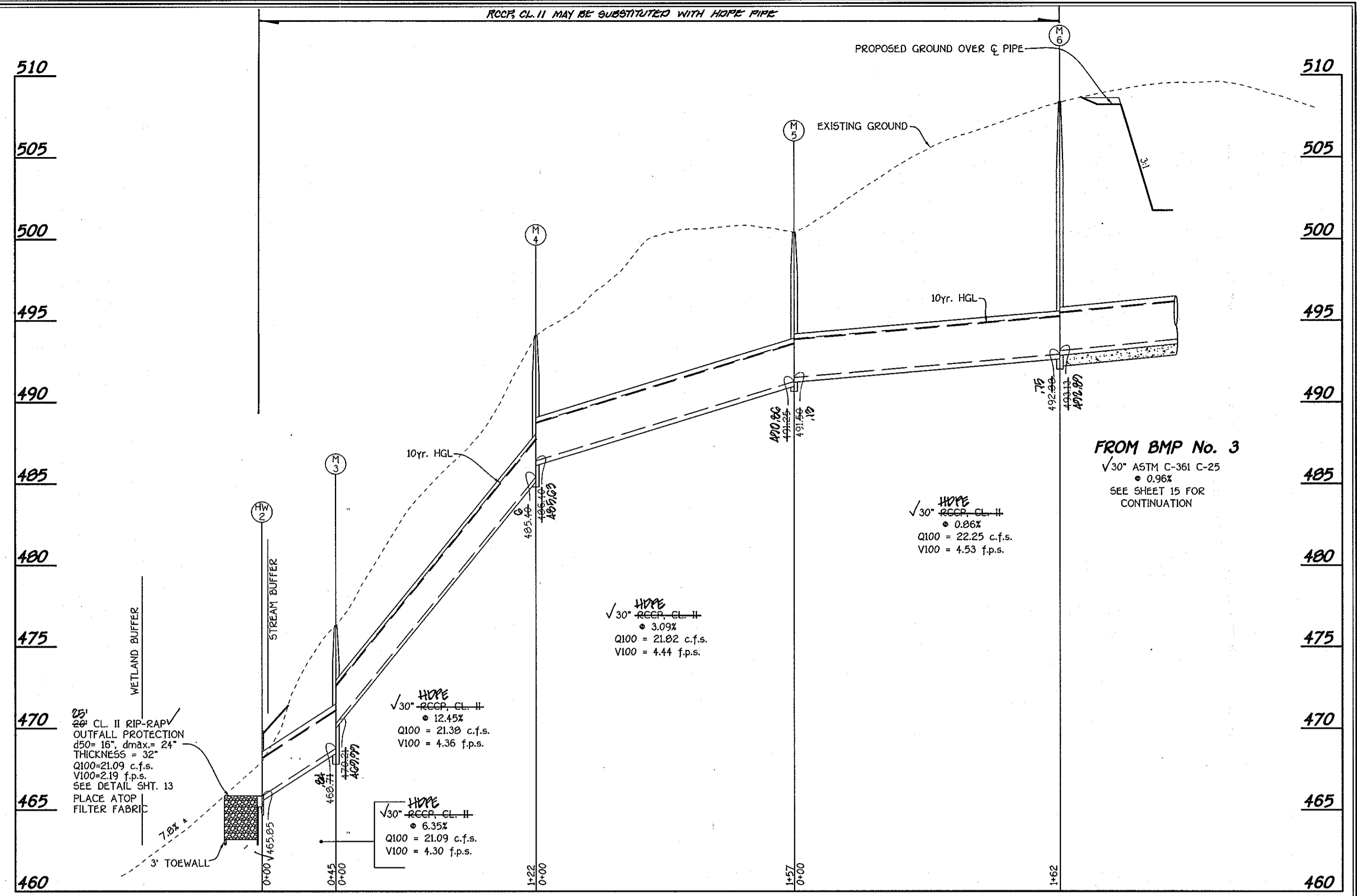


**PROFILE**  
 SCALE: HORIZ. : 1" = 50'  
 VERT. : 1" = 5'

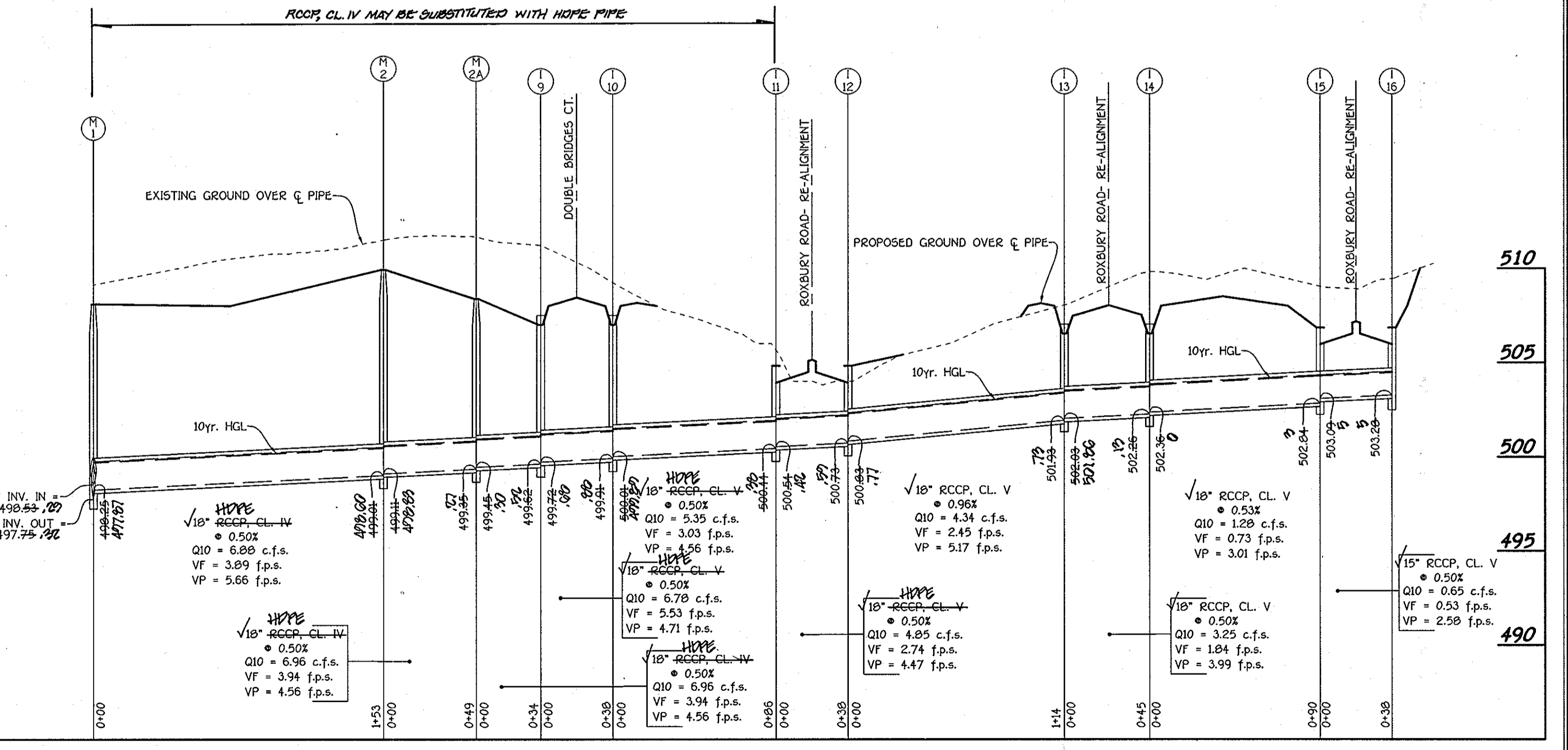
No.	Description	Date
1	Added Stormdrain Pipe Substitution Note	12-1-10
	Revisions	

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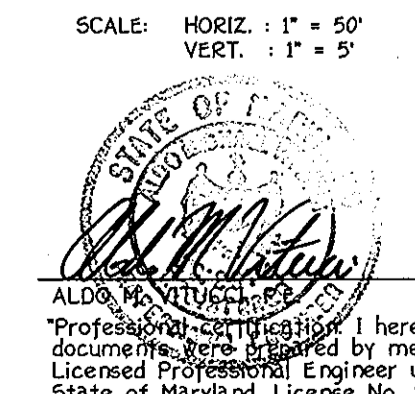
**DEVELOPER**  
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 LISBON, MARYLAND 21765  
 (410) 489-7900



**PROFILE**  
 SCALE: HORIZ. : 1" = 50'  
 VERT. : 1" = 5'



**PROFILE**  
 SCALE: HORIZ. : 1" = 50'  
 VERT. : 1" = 5'



**STORM DRAIN PROFILES**  
**MERIWETHER FARM**  
**SECTION ONE**  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'

TAX MAP No. 21 ZONING: RC-100 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 11 OF 20

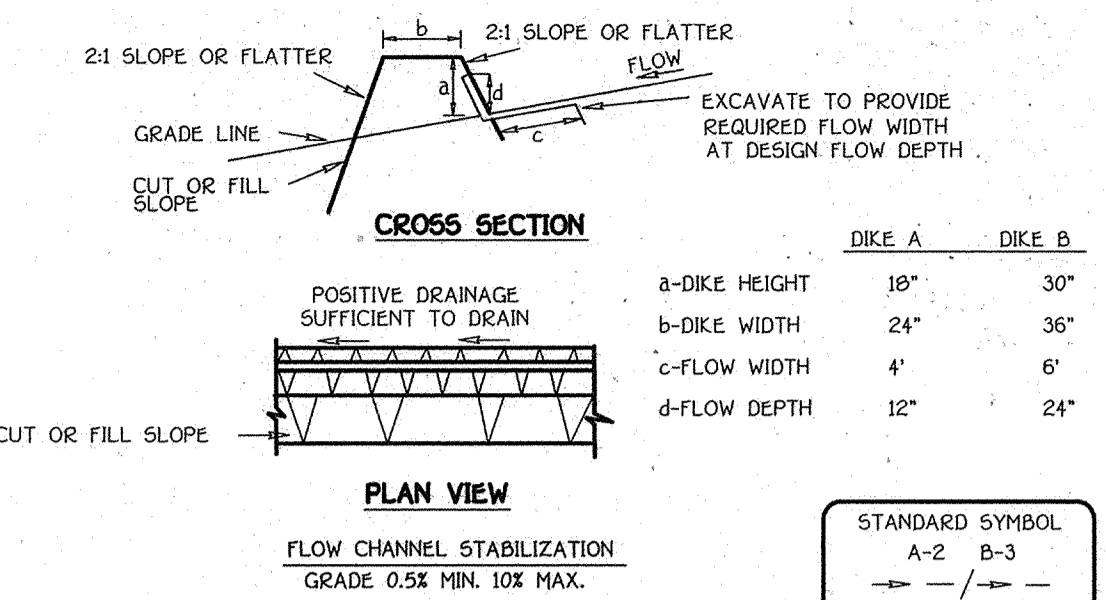
AS BUILT



**ENGINEER'S CERTIFICATE**  
 I hereby certify that this Plan for Erosion and Sediment Control represents a typical and workable plan based on my personal knowledge of the site conditions and that it was prepared in accordance with the requirements of the Howard Soil Conservation District.  
 Signature: *[Signature]* Date: 12-22-06

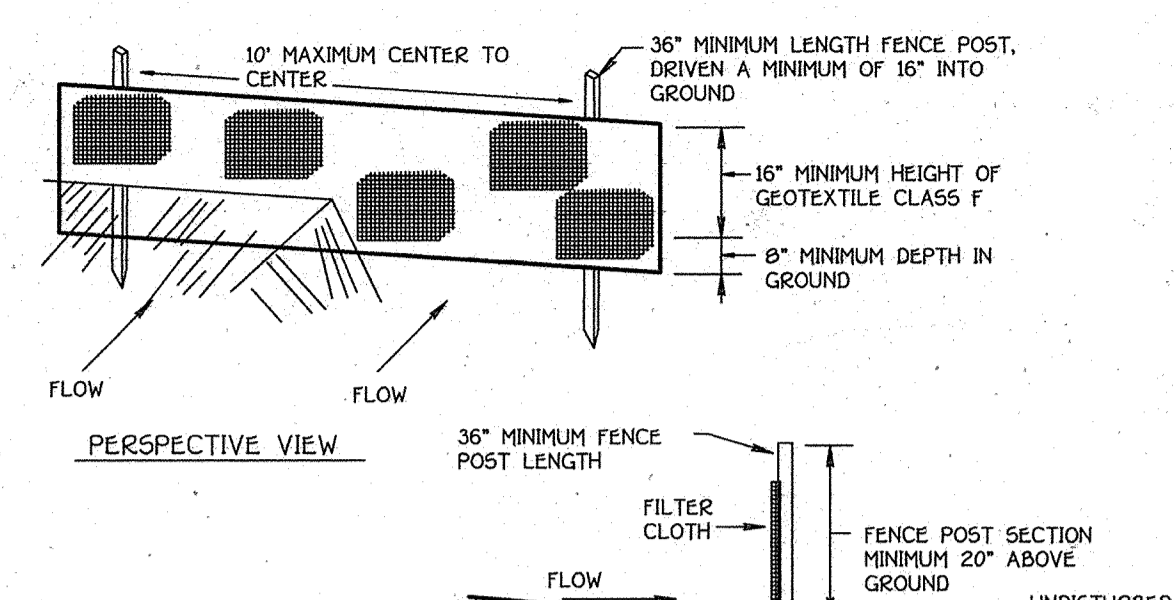
**DEVELOPER'S CERTIFICATE**  
 I/We Certify that all Development and Construction will be done according to this Plan of Development and Plan for Erosion and Sediment Control and that all Responsible Personnel involved in the Construction Project will have a Certificate of Attendance at a Department of Natural Resources Approved Training Program for the Control of Sediment and Erosion before beginning the Project. I also authorize periodic on-site inspection by the Howard Soil Conservation District or their authorized agents as are deemed necessary.  
 Signature of Developer: *[Signature]* Date: 12-22-08

Approved: This Development is Approved for Erosion and Sediment Control by the Howard Soil Conservation District.  
 District Howard Soil Conservation Dist. Date: 1/14/09  
 Approved: Department of Planning and Zoning  
 Chief, Division of Land Development Date: 2/5/09  
 Chief, Development Engineering Division Date: 2/14/09  
 Approved: Howard County Department of Public Works  
 Chief, Bureau of Highways Date: 1-22-09



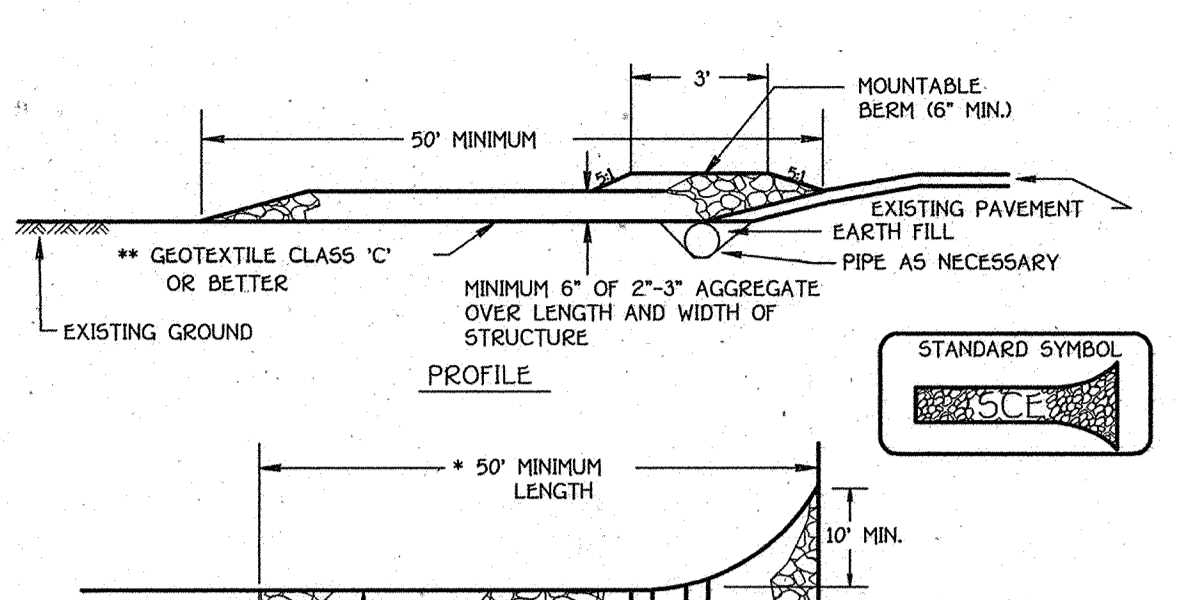
- CONSTRUCTION SPECIFICATIONS**
- All temporary earth dikes shall have uninterrupted positive grade to an outlet. Spot elevations may be necessary for grades less than 1%.
  - Runoff diverted from a disturbed area shall be conveyed to a sediment trapping device.
  - Runoff diverted from an undisturbed area shall outlet directly into an undisturbed, stabilized area at a non-erosive velocity.
  - All trees, brush, stumps, obstructions, and other objectionable material shall be removed and disposed of so as not to interfere with the proper functioning of the dike.
  - The dike shall be excavated or shaped to line, grade and cross section as required to meet the criteria specified herein and be free of back projections or other irregularities which will impede normal flow.
  - Fill shall be compacted by earth moving equipment.
  - All earth removed and not needed for construction shall be placed so that it will not interfere with the functioning of the dike.
  - Dike and maintenance must be provided periodically and after each rain event.

**EARTH DIKE**  
 NOT TO SCALE



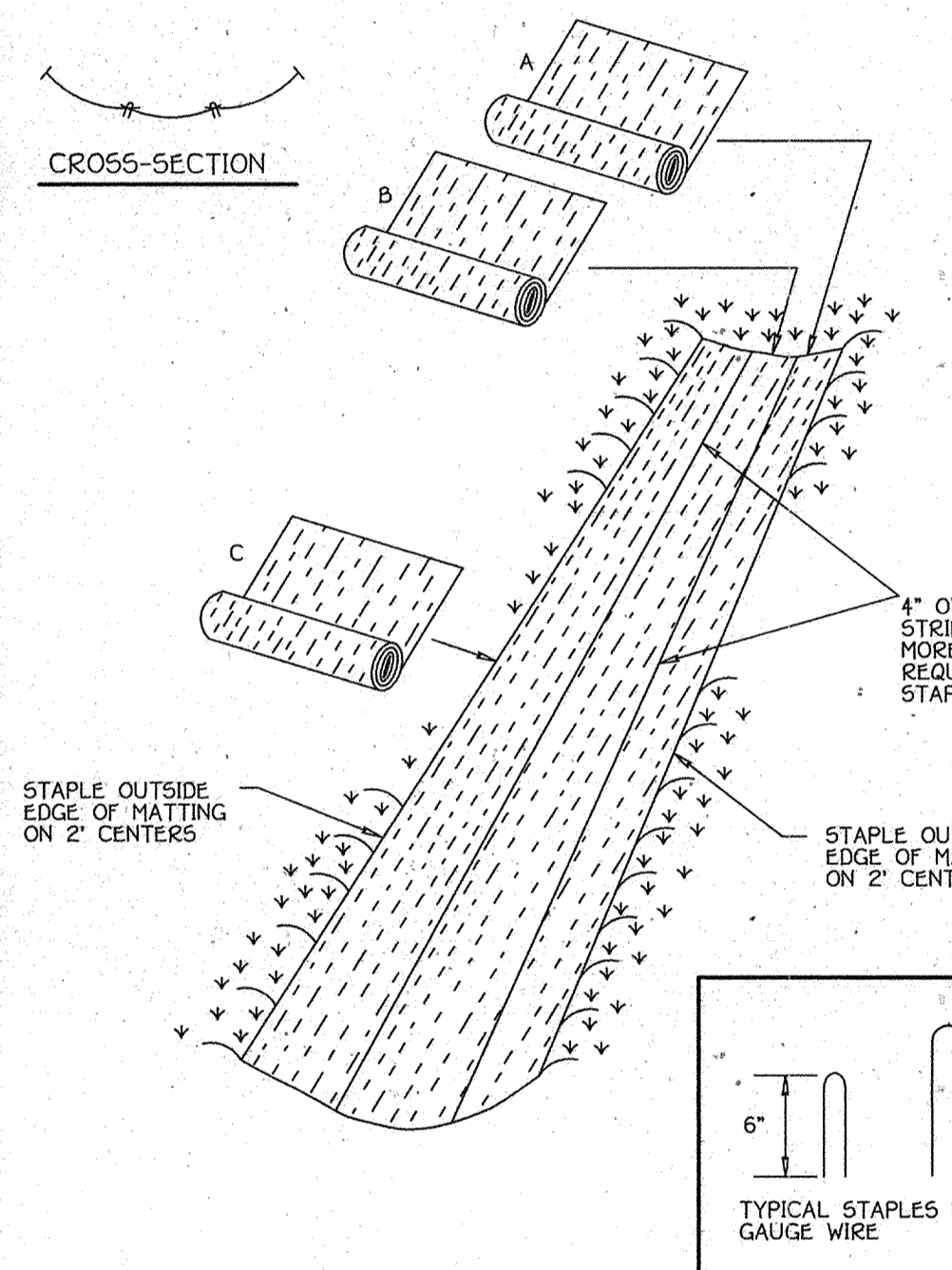
- CONSTRUCTION SPECIFICATIONS**
- Fence posts shall be a minimum of 36" long driven 16" minimum into the ground. Wood posts shall be 1 1/2" x 1 1/2" square (minimum) cut, or 1 3/4" diameter (minimum) round and shall be of sound quality hardwood. Steel posts will be standard I or U section weighing not less than 1.00 pound per linear foot.
  - Geotextile shall be fastened securely to each fence post with wire ties or staples at top and mid-section and shall meet the following requirements for Geotextile Class F:  
 Tensile Strength: 50 lbs/in (min) Test: MSMT 509  
 Tensile Modulus: 20 lbs/in (min) Test: MSMT 509  
 Flow Rate: 0.3 gal ft<sup>2</sup> / minute (max) Test: MSMT 322  
 Filtering Efficiency: 75% (min) Test: MSMT 322
  - Where ends of geotextile fabric come together, they shall be overlapped, folded and stapled to prevent sediment bypass.
  - Silt Fence shall be inspected after each rainfall event and maintained when bulges occur or when sediment accumulation reached 50% of the fabric height.

**SILT FENCE**  
 NOT TO SCALE



- CONSTRUCTION SPECIFICATION**
- Length - minimum of 50' (\*30' for single residence lot).
  - Width - 10' minimum, should be flared at the existing road to provide a turning radius.
  - Geotextile fabric (filter cloth) shall be placed over the existing ground prior to placing stone. \*\*The plan approval authority may not require single family residences to use geotextile.
  - Stone - crushed aggregate (2" to 3") or reclaimed or recycled concrete equivalent shall be placed at least 6" deep over the length and width of the entrance.
  - Surface Water - all surface water flowing to or diverted toward construction entrances shall be piped through the entrance, maintaining positive drainage. Pipe installed through the stabilized construction entrance shall be protected with a mountable berm with 5:1 slopes and a minimum of 6" of stone over the pipe. Pipe has to be sized according to the drainage. When the SCE is located at a high spot and has no drainage to convey a pipe will not be necessary. Pipe should be sized according to the amount of runoff to be conveyed. A 6" minimum will be required.
  - Location - A stabilized construction entrance shall be located at every point where construction traffic enters or leaves a construction site. Vehicles leaving the site must travel over the entire length of the stabilized construction entrance.

**STABILIZED CONSTRUCTION ENTRANCE**  
 NOT TO SCALE



- CONSTRUCTION SPECIFICATIONS**
- Key-in the matting by placing the top ends of the matting in a narrow trench, 6" in depth. Backfill the trench and tamp firmly to conform to the channel cross-section. Secure with a row of staples about 4" down slope from the trench. Spacing between staples is 6".
  - Staple the 4" overlap in the channel center using an 18" spacing between staples.
  - Before stapling the outer edges of the matting, make sure the matting is smooth and in firm contact with the soil.
  - Staples shall be placed 2' apart with 4 rows for each strip, 2 outer rows, and 2 alternating rows down the center.
  - Where one roll of matting ends and another begins, the end of the top strip shall overlap the upper end of the lower strip by 4" shiplap fashion. Reinforce the overlap with a double row of staples spaced 6" apart in a staggered pattern on either side.
  - The discharge end of the matting liner should be similarly secured with 2 double rows of staples.
- Note: If flow will enter from the edge of the matting then the area effected by the flow must be keyed-in.

**EROSION CONTROL MATTING**  
 NOT TO SCALE

**FISHER, COLLINS & CARTER, INC.**  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTENNIAL SQUARE OFFICE PARK - 8072 BALTIMORE NATIONAL PIKE  
 ELICOTT CITY, MARYLAND 21042  
 (410) 461-2955

**OWNER**  
 FRANCES B. DEVLIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 489-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
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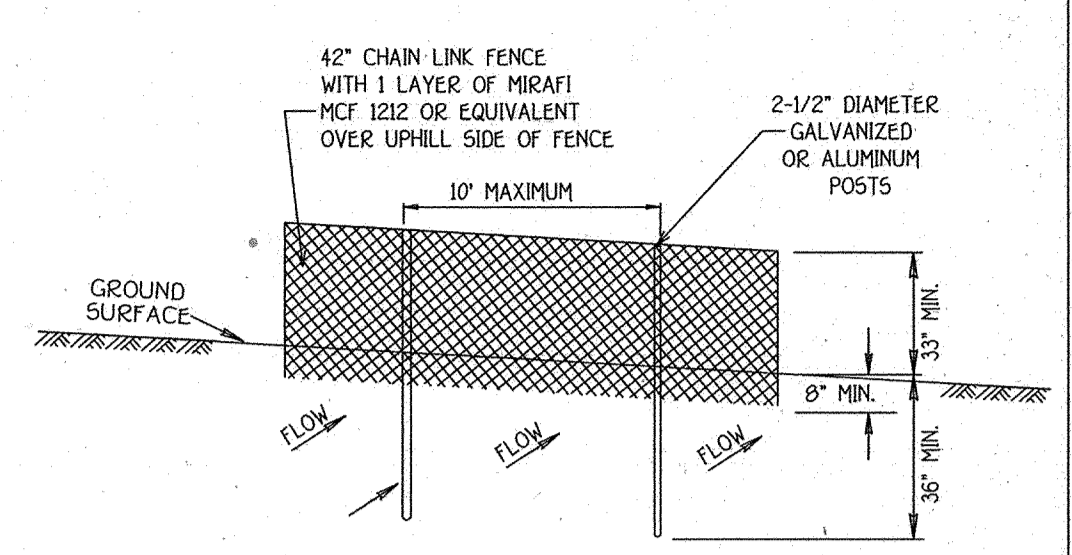
**CONSTRUCTION SPECIFICATIONS**

- FENCING SHALL BE 42" HIGH CHAIN LINK CONSTRUCTED IN ACCORDANCE WITH THE LATEST MARYLAND STATE HIGHWAY ADMINISTRATION STANDARD DETAILS 690.01 AND 690.02 FOR CHAIN U FENCING. THE SPECIFICATIONS FOR A 6'-0" FENCE SHALL BE USED, SUBSTITUTING 42" FABRIC AND 8" POSTS. POSTS SHALL BE PLACED WITHOUT CONCRETE EMBEDMENT.
- CHAIN LINK FENCE SHALL BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES. THE LOWER TENSION WIRE, BRACE AND TRUSS RODS, ANCHORS AND POST CAPS ARE NOT REQUIRED EXCEPT ON THE ENDS OF THE FENCE.
- FILTER CLOTH TO BE FASTENED SECURELY TO CHAIN LINK FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID SECTION.
- FILTER CLOTH SHALL BE IMBEDDED A MINIMUM OF 9" INTO THE GROUND.
- WHEN TWO SECTIONS OF DIVERSION CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY SIX INCHES AND FOLDED.
- MAINTENANCE SHALL BE PERFORMED AS NEEDED.

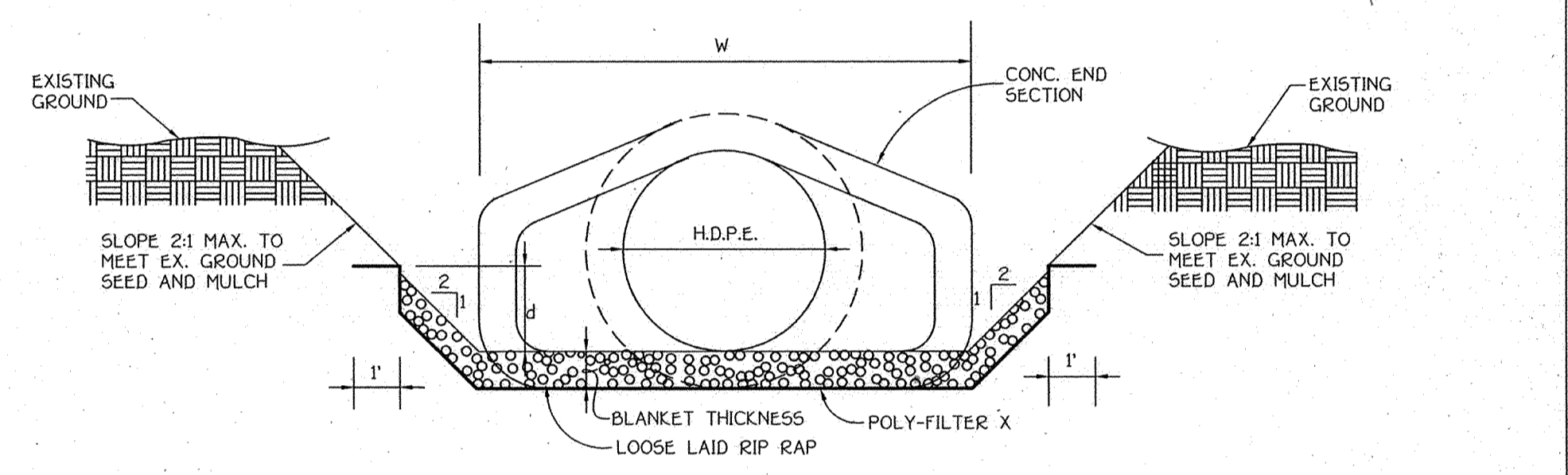
Fabric Properties	Value	Test Method
Grab Tensile Strength (lbs)	90	ASTM D682
Elongation at Failure (%)	50	ASTM D682
Mullen Burst Strength (PSI)	190	ASTM D3786
Puncture Strength (lbs)	40	ASTM D751
Slurry Flow Rate (gal/min/ft <sup>2</sup> )	0.3	Virginia DOT VTM-51
Equivalent Opening Size	40-80	US Std Sieve CW-42225
Ultraviolet Radiation Stability (%)	90	ASTM G-26

**Design Criteria**

Slope	Slope Steepness	Slope Length (maximum)	Silt Fence Length (maximum)
0 - 10%	0 - 10%	Unlimited	Unlimited
10 - 20%	10 - 5%	400 feet	1500 feet
20 - 33%	5 - 3%	300 feet	1000 feet
33 - 50%	3 - 2%	200 feet	500 feet
50% +	2% +	100 feet	250 feet



**SUPER SILT FENCE DETAIL**  
 NOT TO SCALE



**RIP RAP CHANNEL DETAIL**  
 NO SCALE

**RIP-RAP CHANNEL DESIGN DATA**

LENGTH	STRUCTURE	AREA	WETTED PERIMETER	R	R <sup>2/3</sup>	S	S <sup>1/2</sup>	W	d	N	V (f.p.s.)	Q (c.f.s.)	BLANKET THICKNESS
20'	HW-1	15.33'	15.50'	0.9890	0.9926	0.0050	0.0707	10.0'	1.23'	0.04	2.61	* 39.4	9.5" 15"
20'	HW-2	9.63'	12.66'	0.7607	0.8326	0.0050	0.0707	10.0'	0.93'	0.04	2.19	* 21.09	16" 24"
STILLING BASIN	S-1	4.99'	7.89'	0.6324	0.7356	0.0050	0.0707	4.0'	0.87'	0.04	1.94	9.49	9.5" 15"
	S-2	2.81'	6.46'	0.4350	0.5725	0.0050	0.0707	4.0'	0.55'	0.04	1.51	4.19	9.5" 15"
	S-3	6.64'	9.29'	0.7147	0.7985	0.0050	0.0707	5.0'	0.96'	0.04	2.10	13.94	9.5" 15"

\* - DENOTES 100-YEAR Q USED FOR HW-1 AND HW-2.

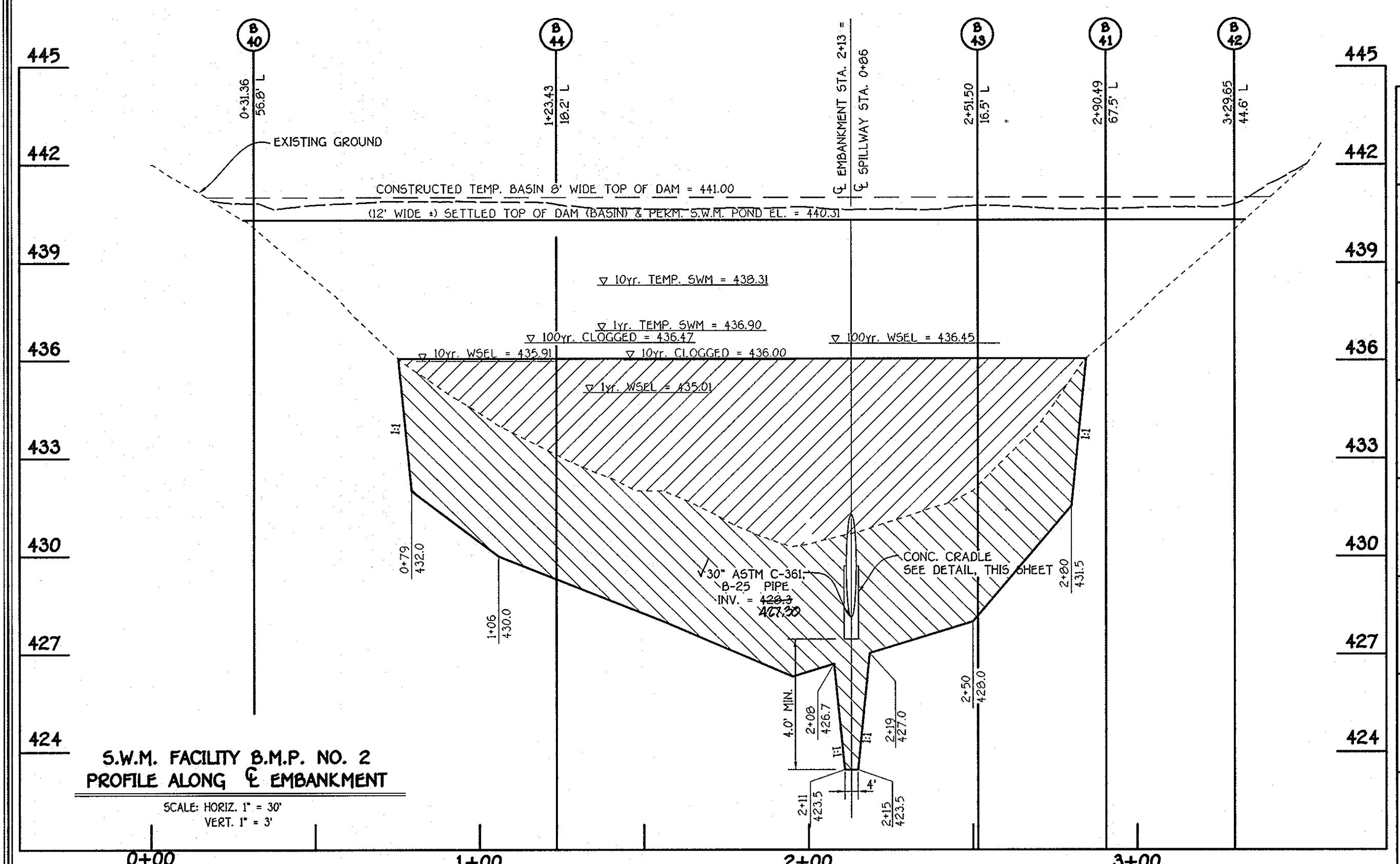
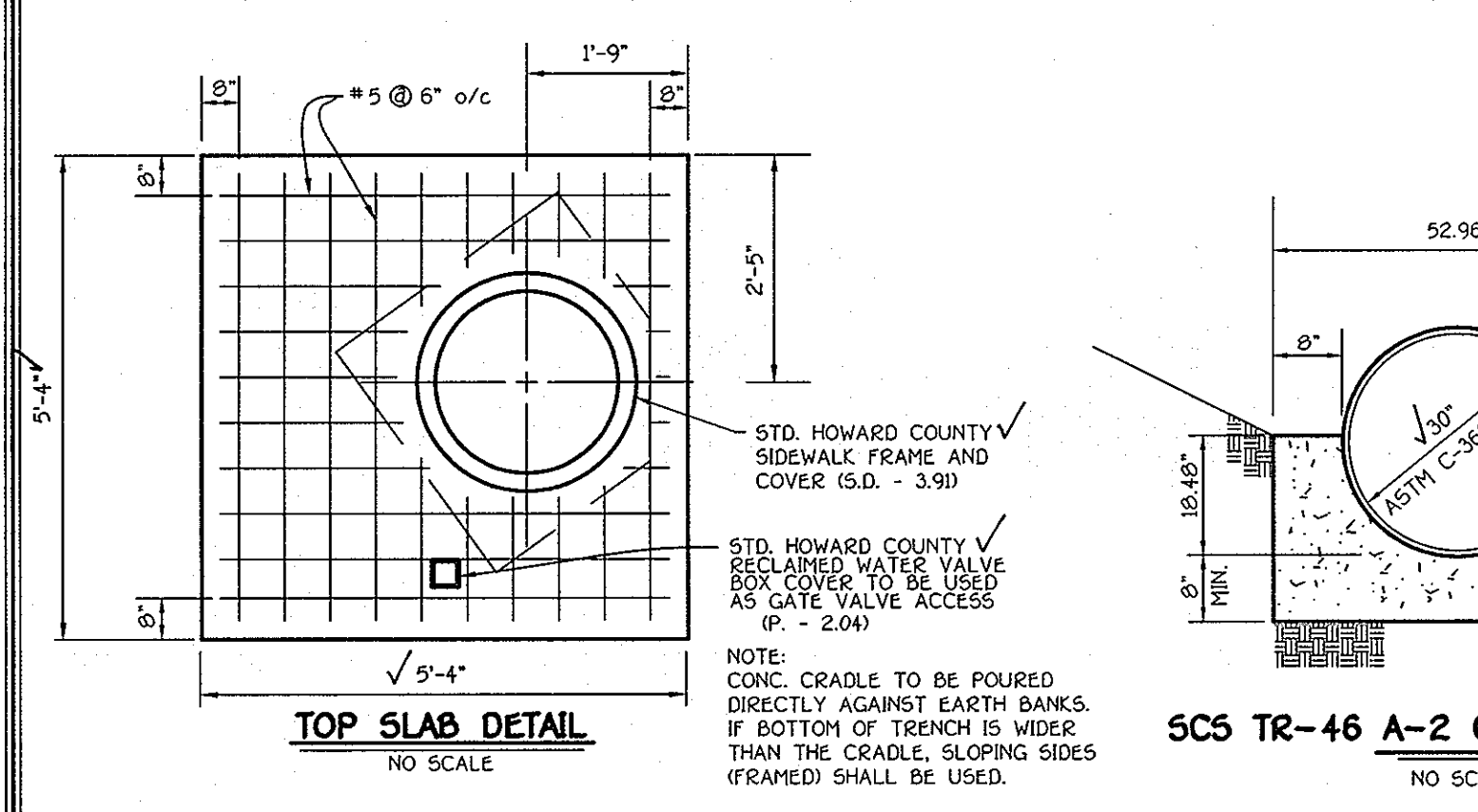
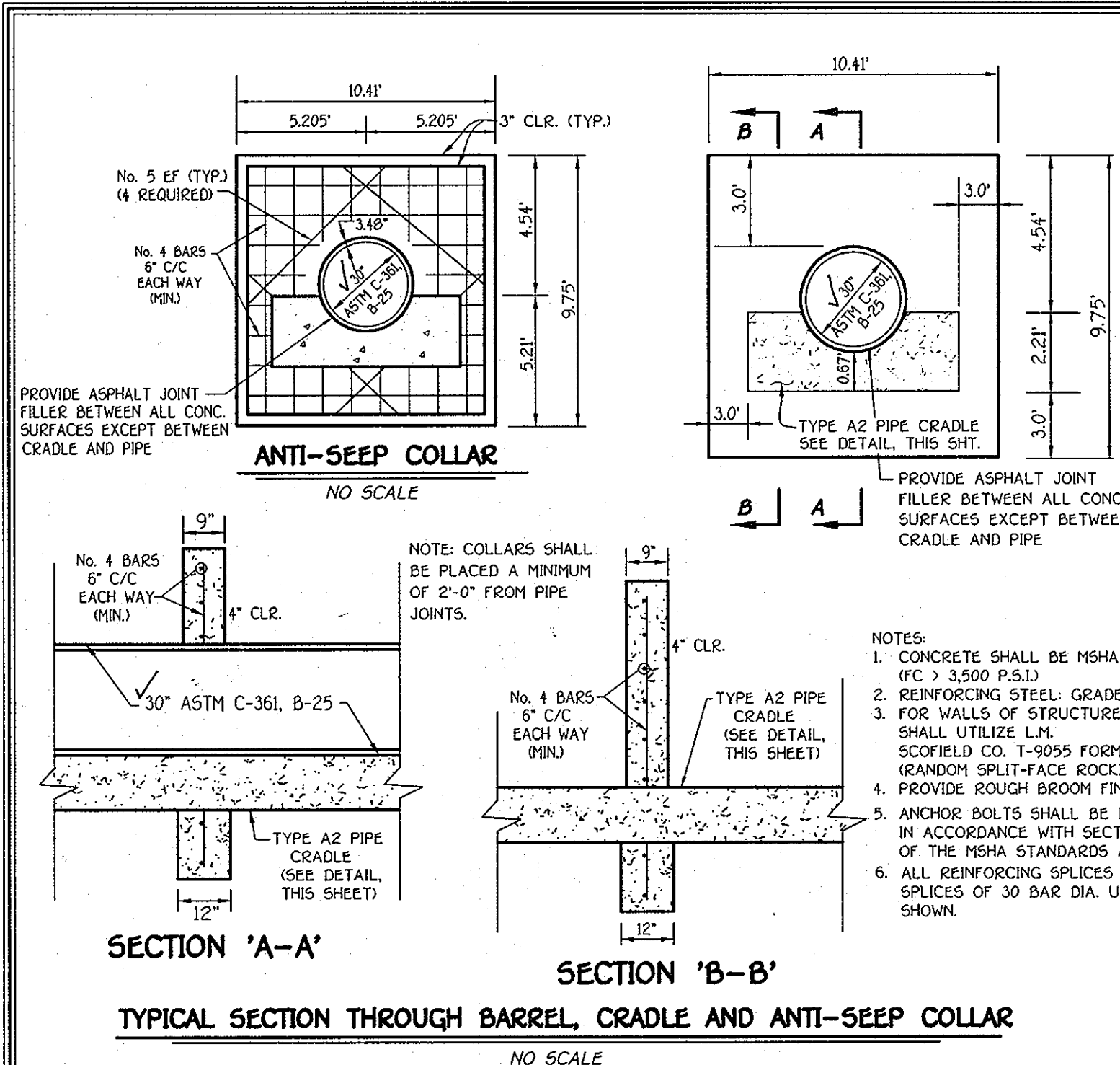
**CONSTRUCTION SPECIFICATIONS FOR RIP-RAP OUTFALLS**

- The subgrade for the filter, riprap or gabion shall be prepared to the required lines and grades. Any fill required in the subgrade shall be compacted to a density of approximately that of the surrounding undisturbed material.
- The rock or gravel shall conform to the specified grading limits when installed respectively in the riprap or filter.
- Filter cloth shall be protected from punching, cutting or tearing. Any damage other than an occasional hole shall be repaired by placing another piece of cloth over the damaged part or by completely replacing the cloth. All overlaps whether for repairs or for joining two pieces of cloth shall be a minimum of one foot.
- Stone for the riprap or gabion outlets may be placed by equipment. Both shall each be constructed to the full course thickness in one operation and in such a manner as to avoid displacement of underlying materials. The stone for riprap or gabion outlets shall be delivered and placed in a manner that will insure that it is reasonably homogenous with the smaller stones and spalls filling the voids between the larger stones. Riprap shall be placed in a manner to prevent damage to the filter blanket or filter cloth. Hand placement will be required to the extent necessary to prevent damage to the permanent works.

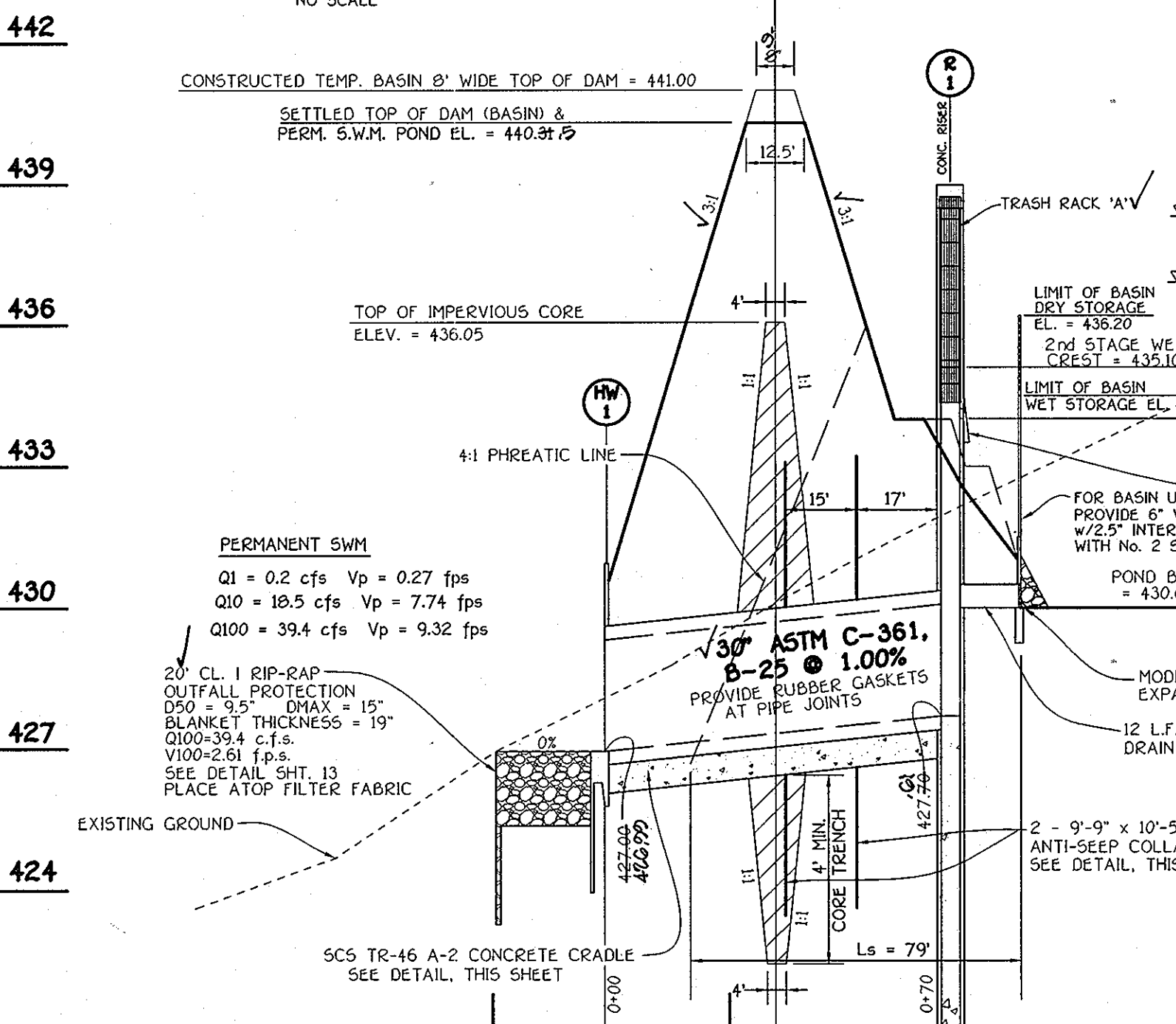
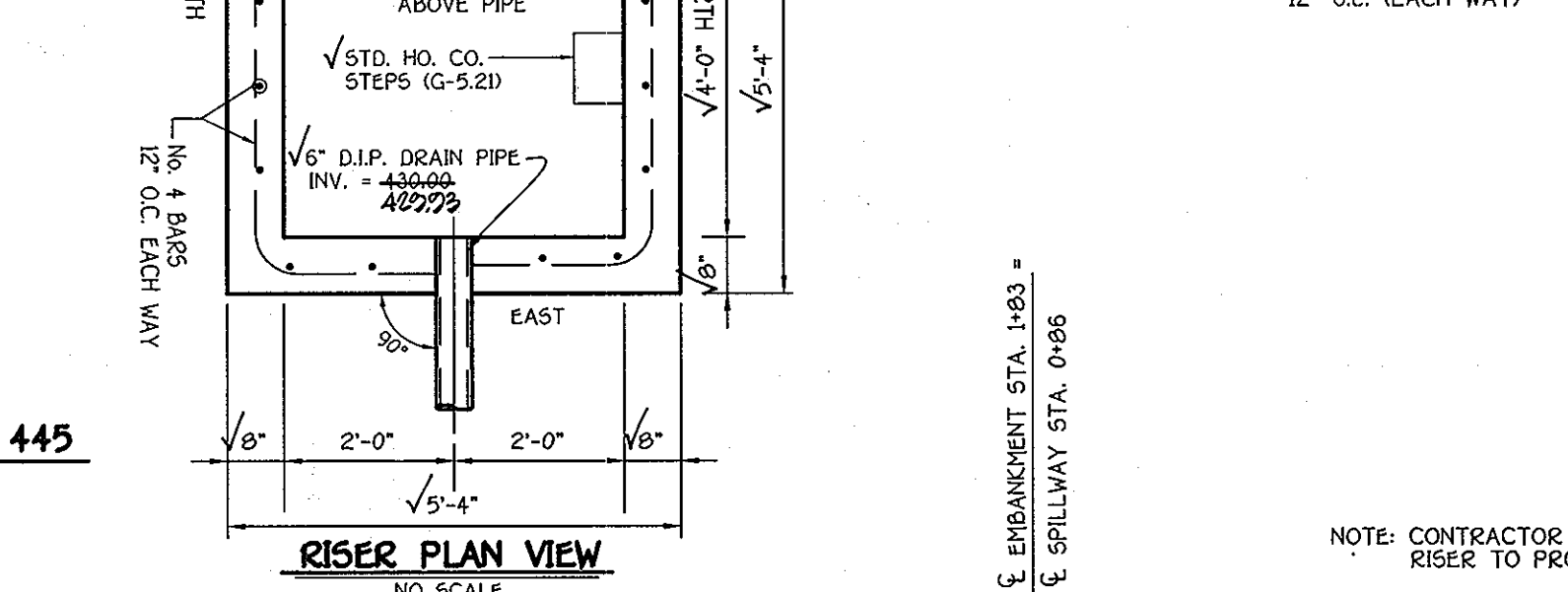
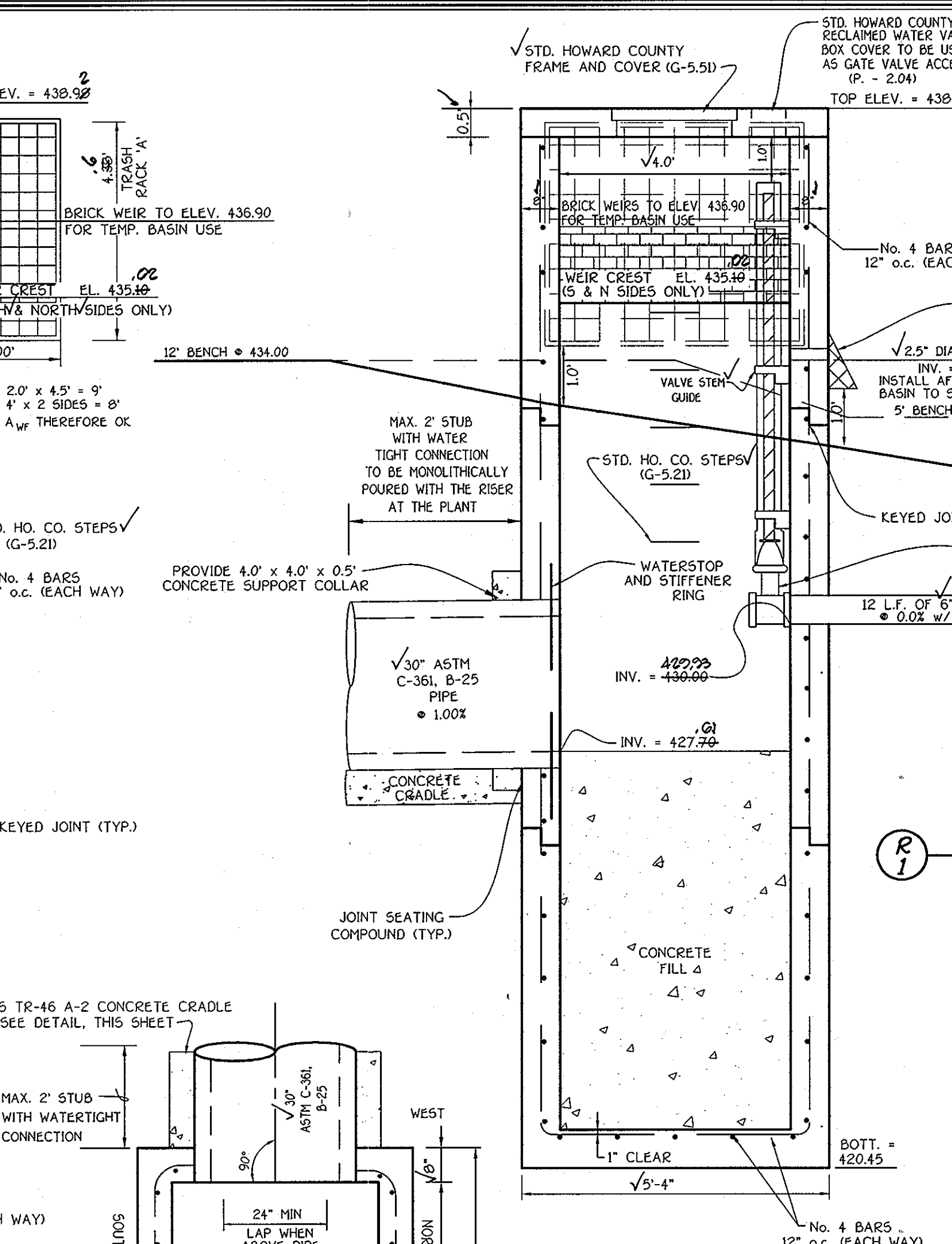
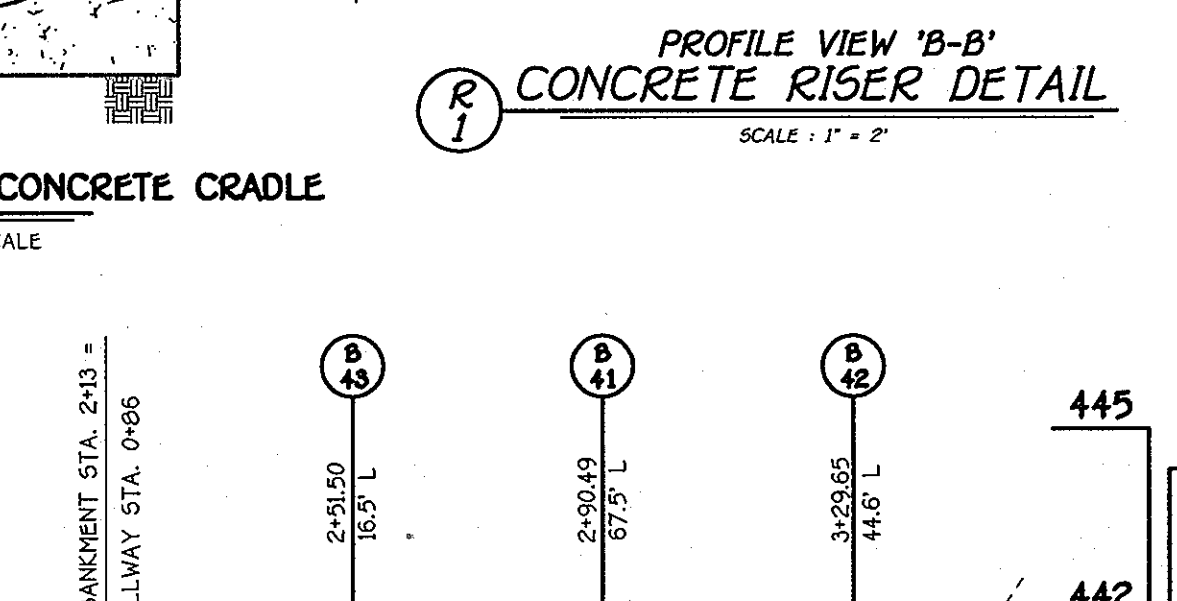
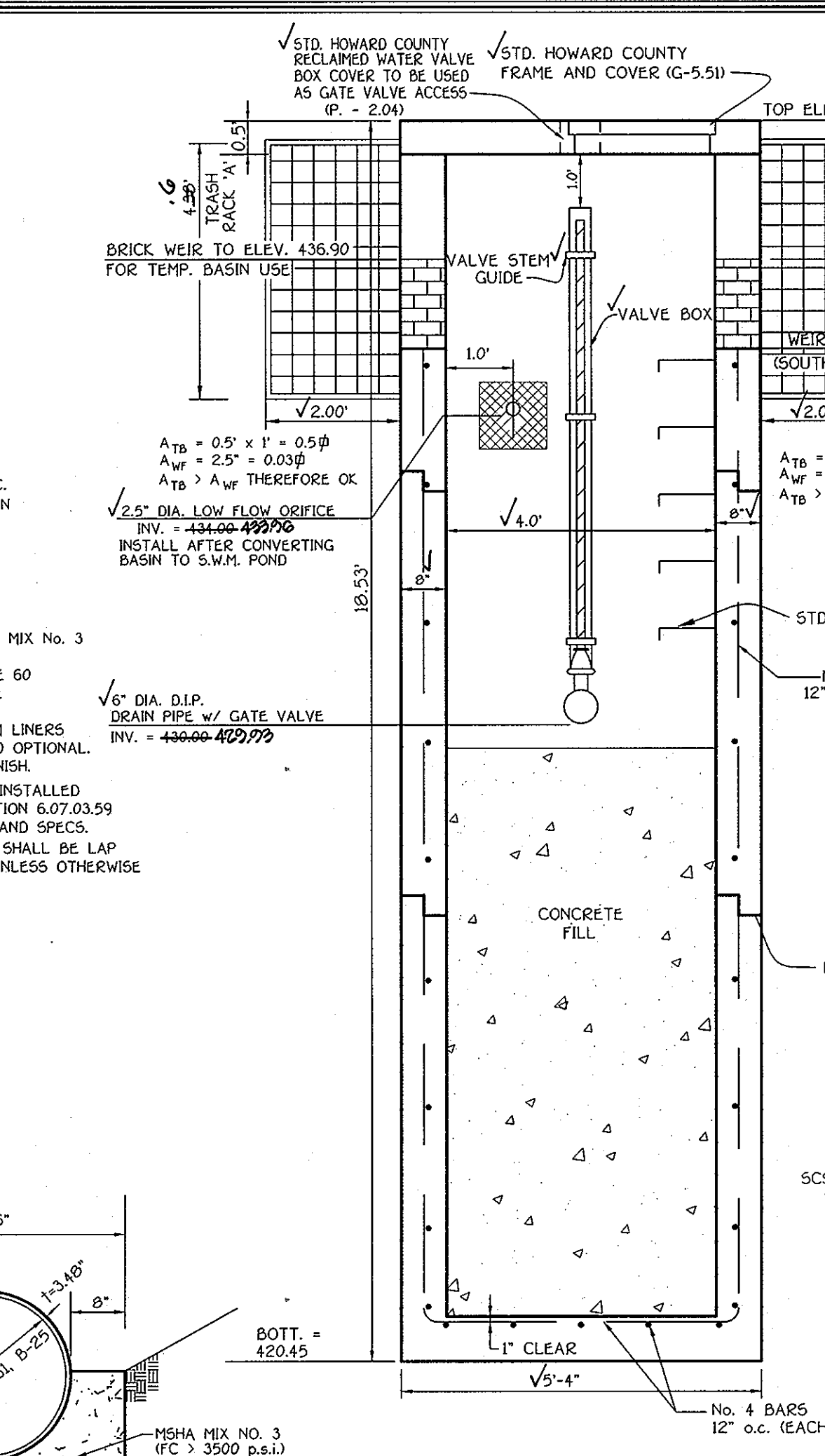
**SEDIMENT CONTROL DETAILS**  
**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D',  
 AND NON-BUILDABLE PARCEL 'E'

**STATE OF MARYLAND**  
 PROFESSIONAL ENGINEER  
 Signature: *[Signature]* Date: 12-22-08  
 I hereby certify that these documents were prepared by me, and that I am a duly Licensed Professional Engineer under the laws of the State of Maryland, License No. 20748, Expiration Date 2-22-09.

ZONED: RC-DEO DATE: 12-22-08  
 TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 DISTRICT: HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 13 OF 20

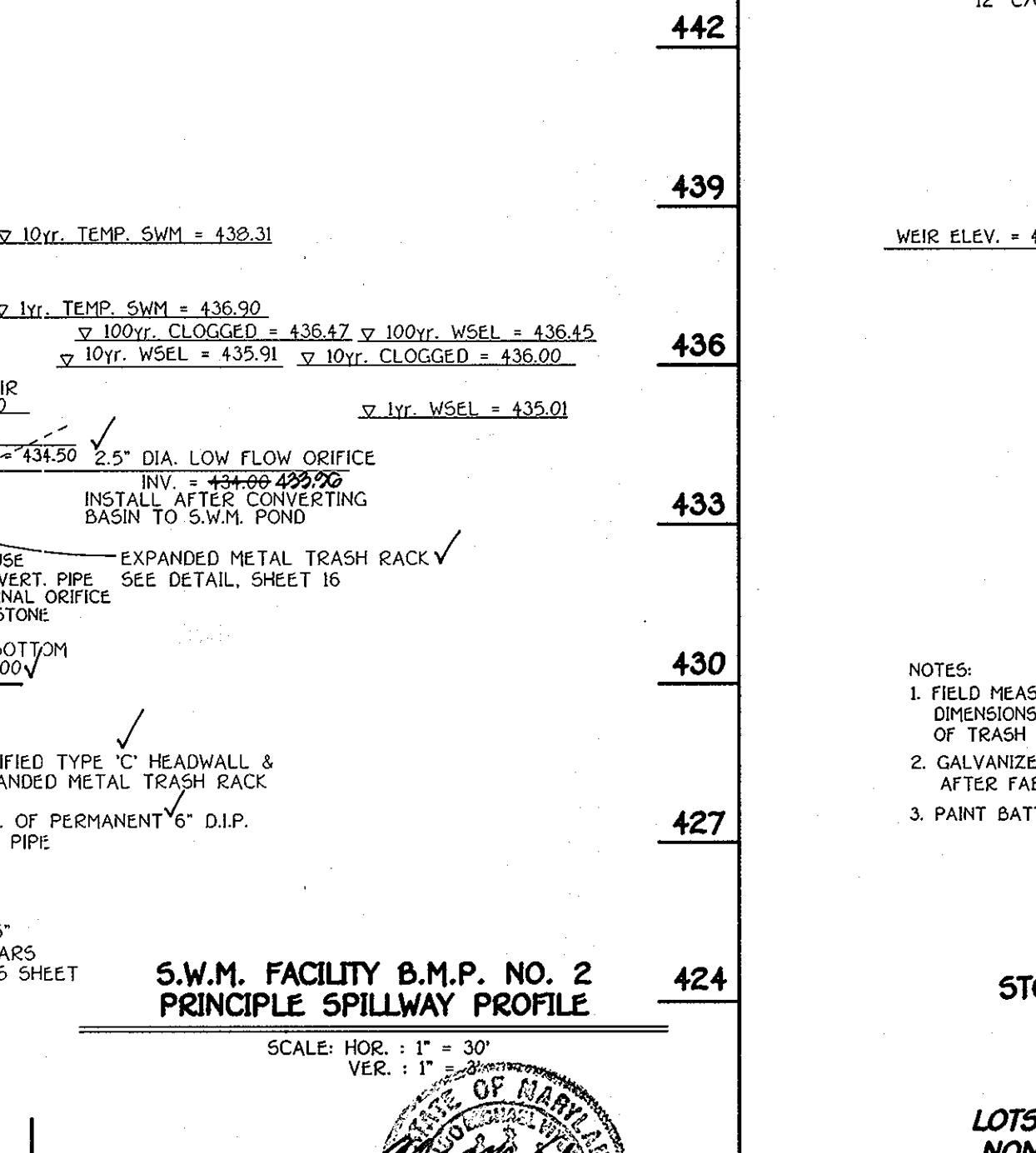
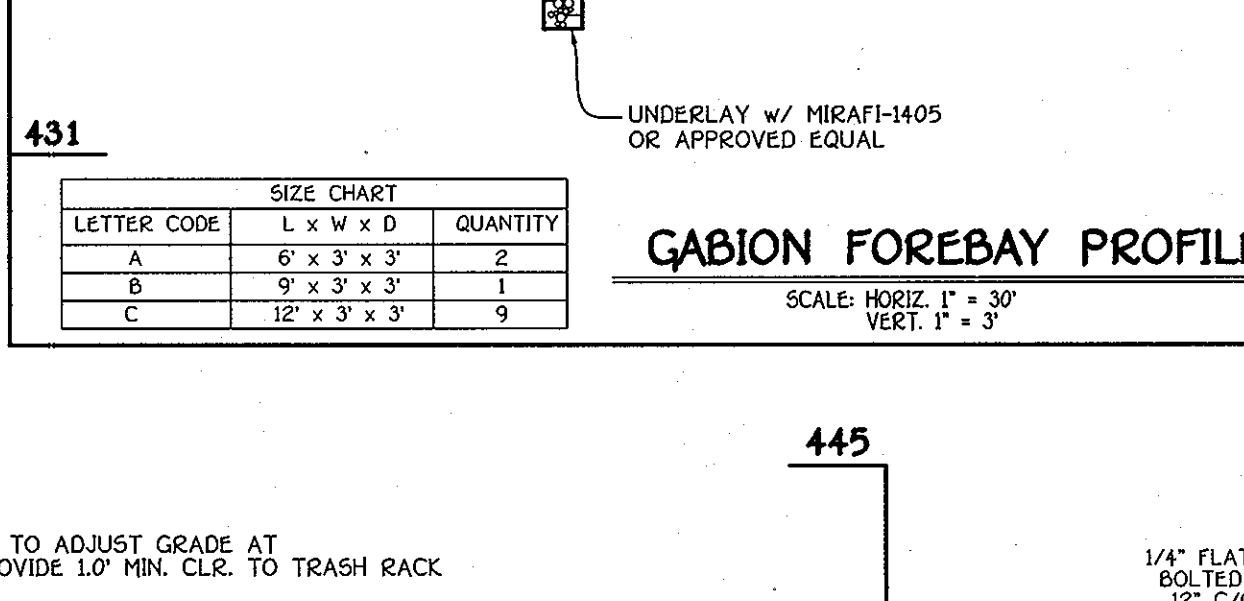
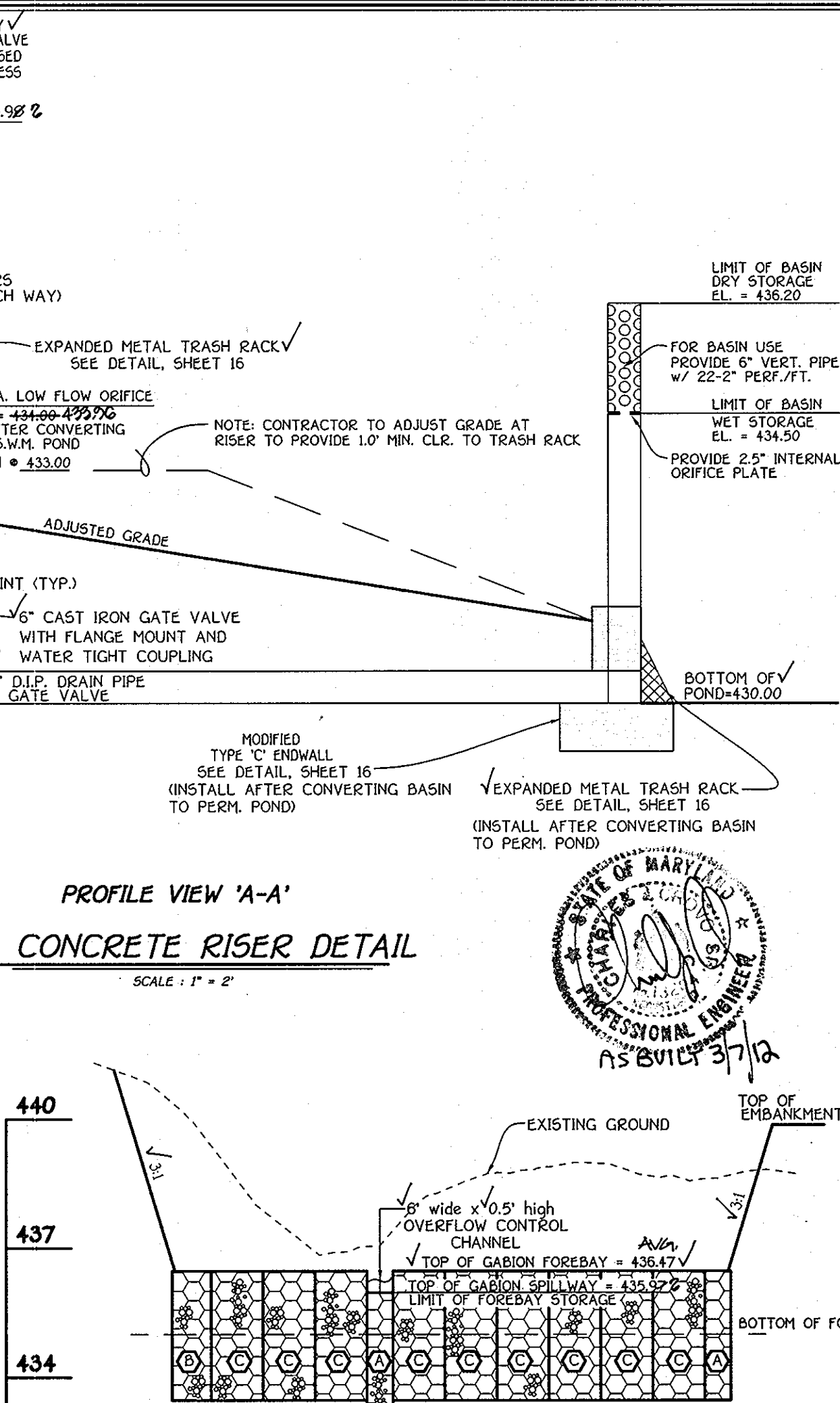


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LITCHFIELD, CT. 06759  
(410) 489-7900

**DEVELOPER**  
HERITAGE LAND DEVELOPMENT  
1950 NORTH AVENUE, P.O. BOX 482  
LISBON, MARYLAND 21765  
(410) 489-7900

By the Developer:  
I/We Certify That All Development And/Or Construction Will Be Done According To These Plans, And That Any Responsible Personnel Involved In The Construction Project Will Have A Certificate Of Attendance At A Department Of The Environment Approved Training Program For The Control Of Sediment And Erosion Before Beginning The Project. I Shall Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion. I Shall Authorize Periodic On-Site Inspections By The Howard Soil Conservation District.

Signature: *David Goodie* Date: 1-14-09  
Printed Name of Developer: David Goodie

By the Engineer:  
I Certify That These Plans For Pond Construction, Erosion And Sediment Control Represents A Practical Application Of My Professional Knowledge Of The Site Conditions. This Plan Was Prepared In Accordance With The Requirements Of The Howard Soil Conservation District. I Have Reviewed The Plans And Have Made Necessary Revisions To Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion.

Signature: *[Signature]* Date: 1-14-09  
Printed Name of Engineer: [Name]

Approved Department of Public Works  
Signature: *[Signature]* Date: 1/16/09  
Chief, Bureau of Highways

Approved Department of Planning And Zoning  
Signature: *[Signature]* Date: 2/5/09  
Chief, Division of Land Development

Signature: *[Signature]* Date: 2/4/09  
Chief, Development Engineering Division

**AS-BUILT CERTIFICATION**  
I Herby Certify That The Facility Shown On This Plan Was Constructed As Shown On The "As-Built" Plans And Meets The Approved Plans And Specifications.

Signature: *[Signature]* Date: 1/28/09  
P.E. No.: 37112

Certify Means To State Or Declare A Professional Opinion Based Upon On-site Inspections And Material Tests Which Are Conducted During Construction. The On-site Inspections And Material Tests Are Those Inspections And Tests Deemed Sufficient And Appropriate Commonly Accepted Engineering Standards. Certify Does Not Mean Or Imply A Guarantee By The Engineer Nor Does An Engineer's Certification Relieve Any Other Party From Meeting Requirements Imposed By Contract, Industry Or Other Means, Including Meeting Commonly Accepted Industry Practices.

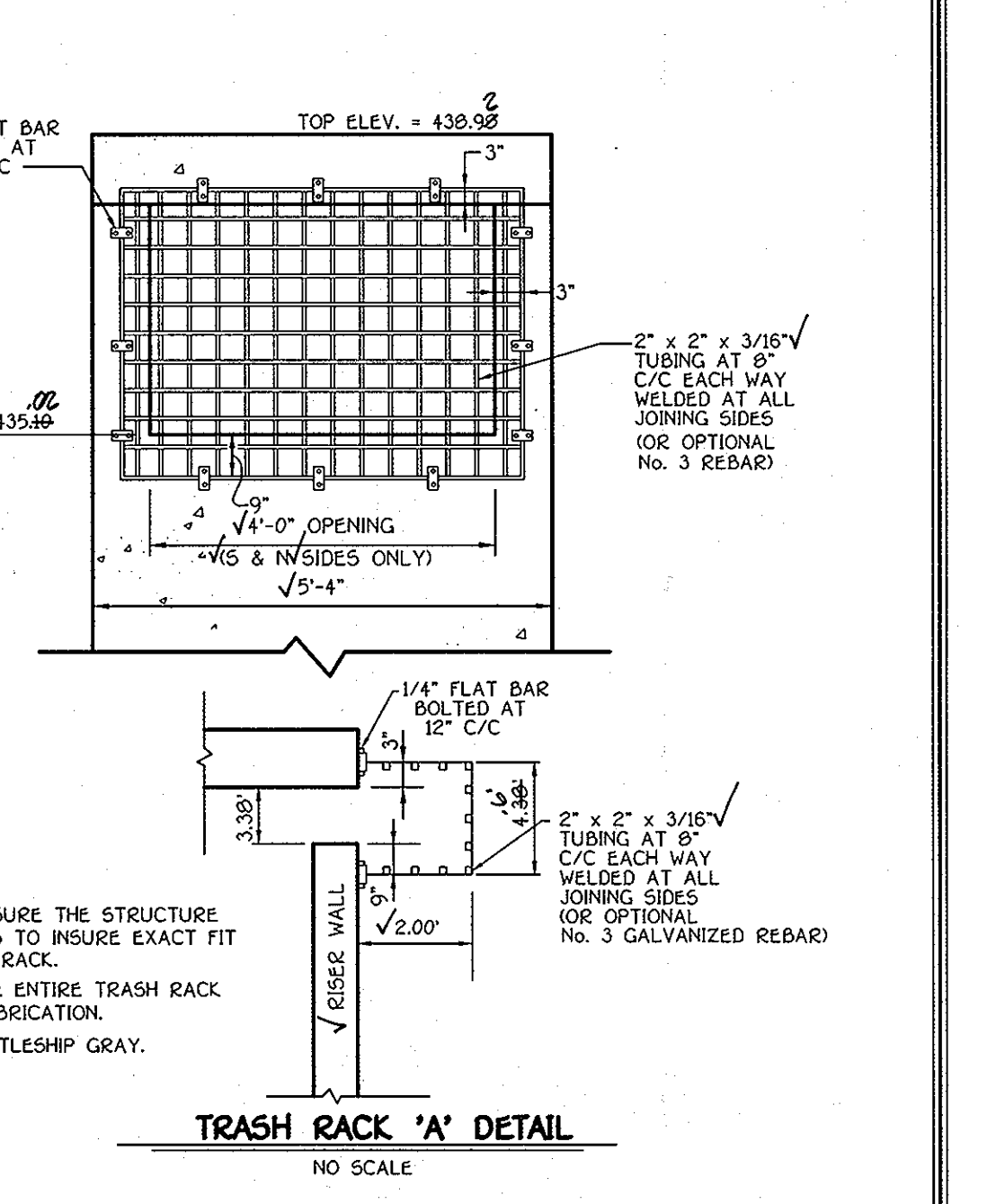
**OPERATION AND MAINTENANCE SCHEDULE FOR PRIVATELY OWNED AND JOINTLY MAINTAINED STORMWATER MANAGEMENT FACILITIES**

**ROUTINE MAINTENANCE**

- Facility shall be inspected annually and after major storms. Inspections shall be performed during wet weather to determine if the pond is functioning properly.
- Top and side slopes of the embankment shall be mowed a minimum of two (2) times a year, once in June and once in September. Other side slopes and maintenance access should be mowed as needed.
- Debris and litter shall be removed during regular mowing operations and as needed.
- Visible signs of erosion in the pond as well as the rip-rap or gabion outlet area shall be repaired as soon as it is noticed.

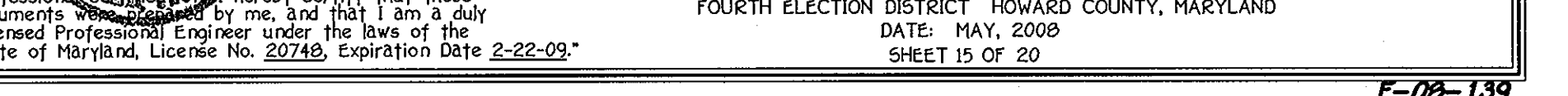
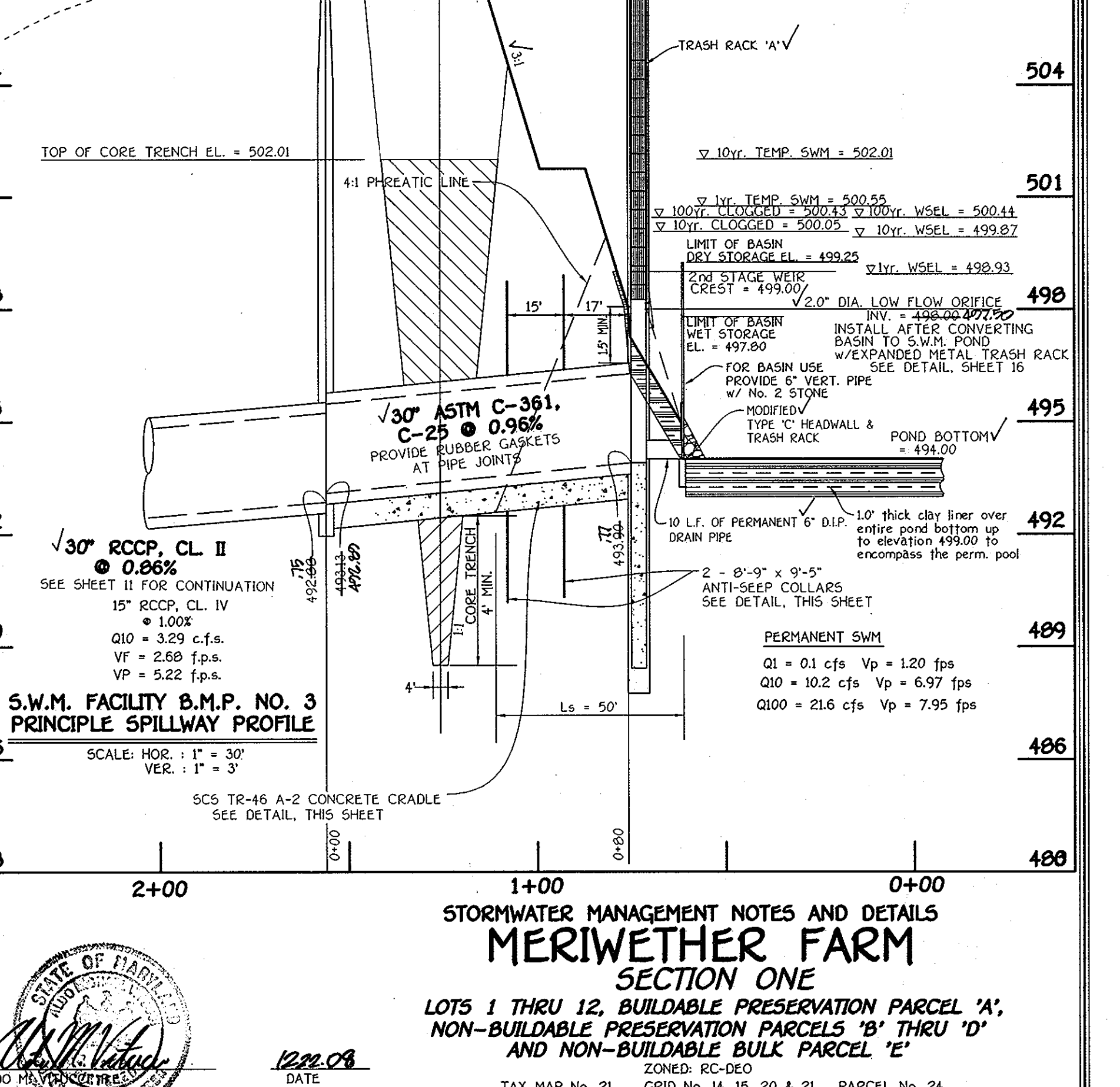
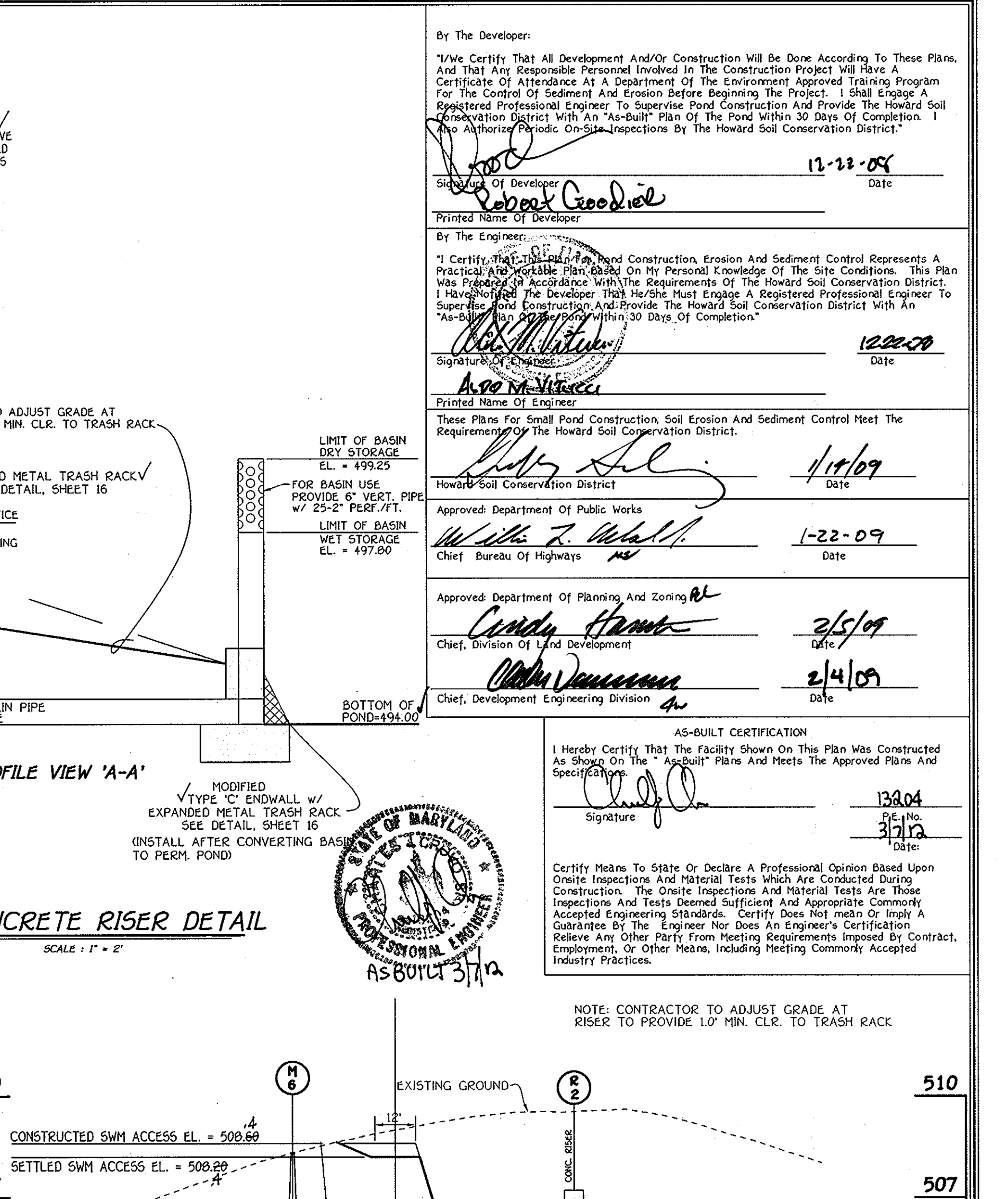
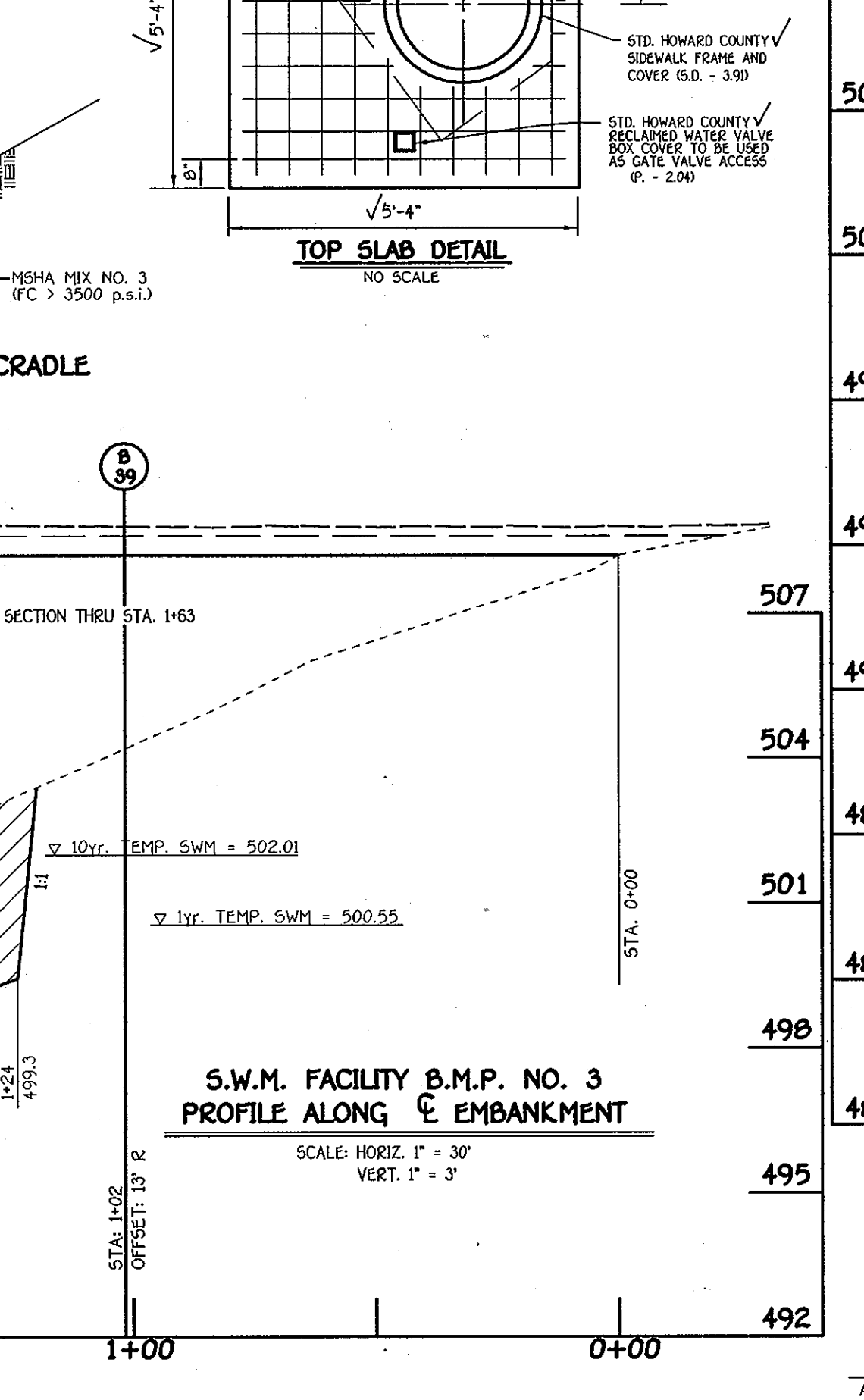
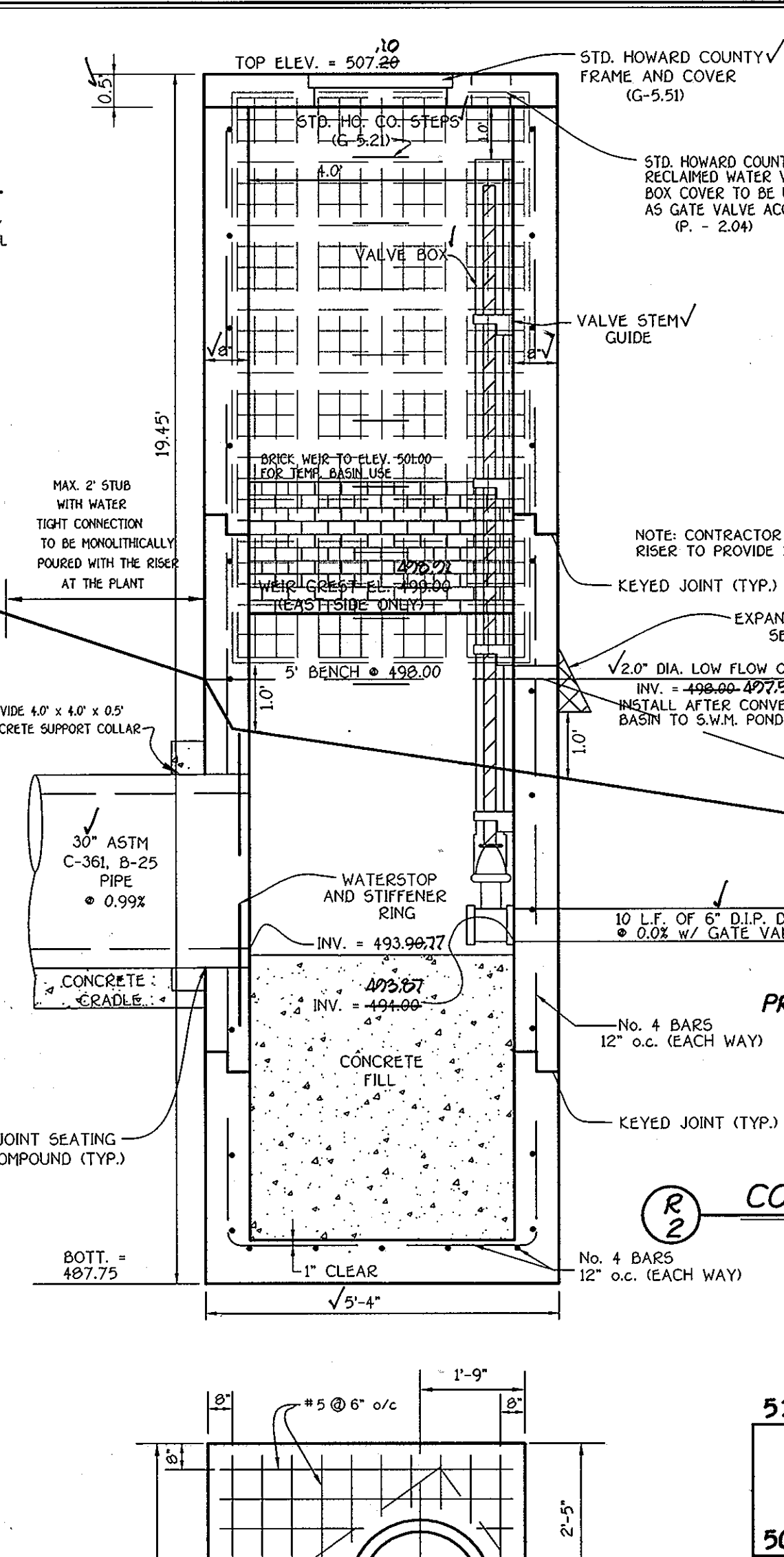
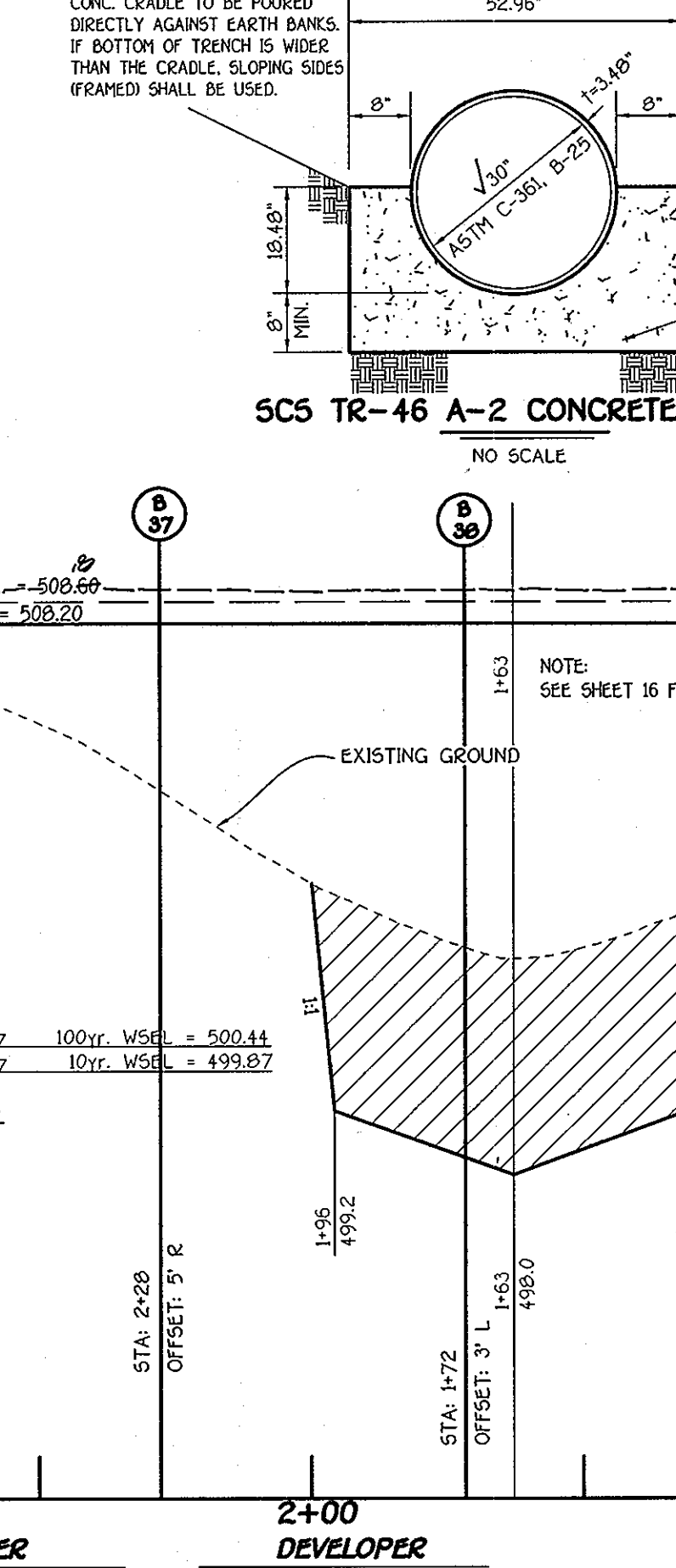
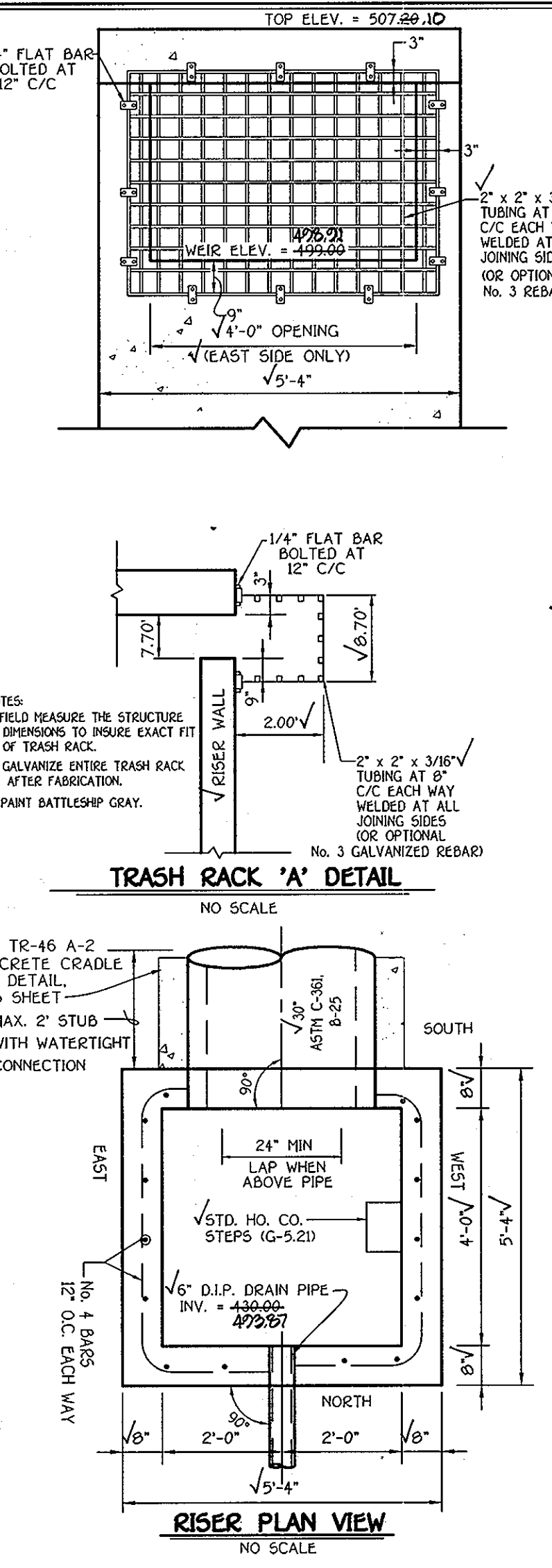
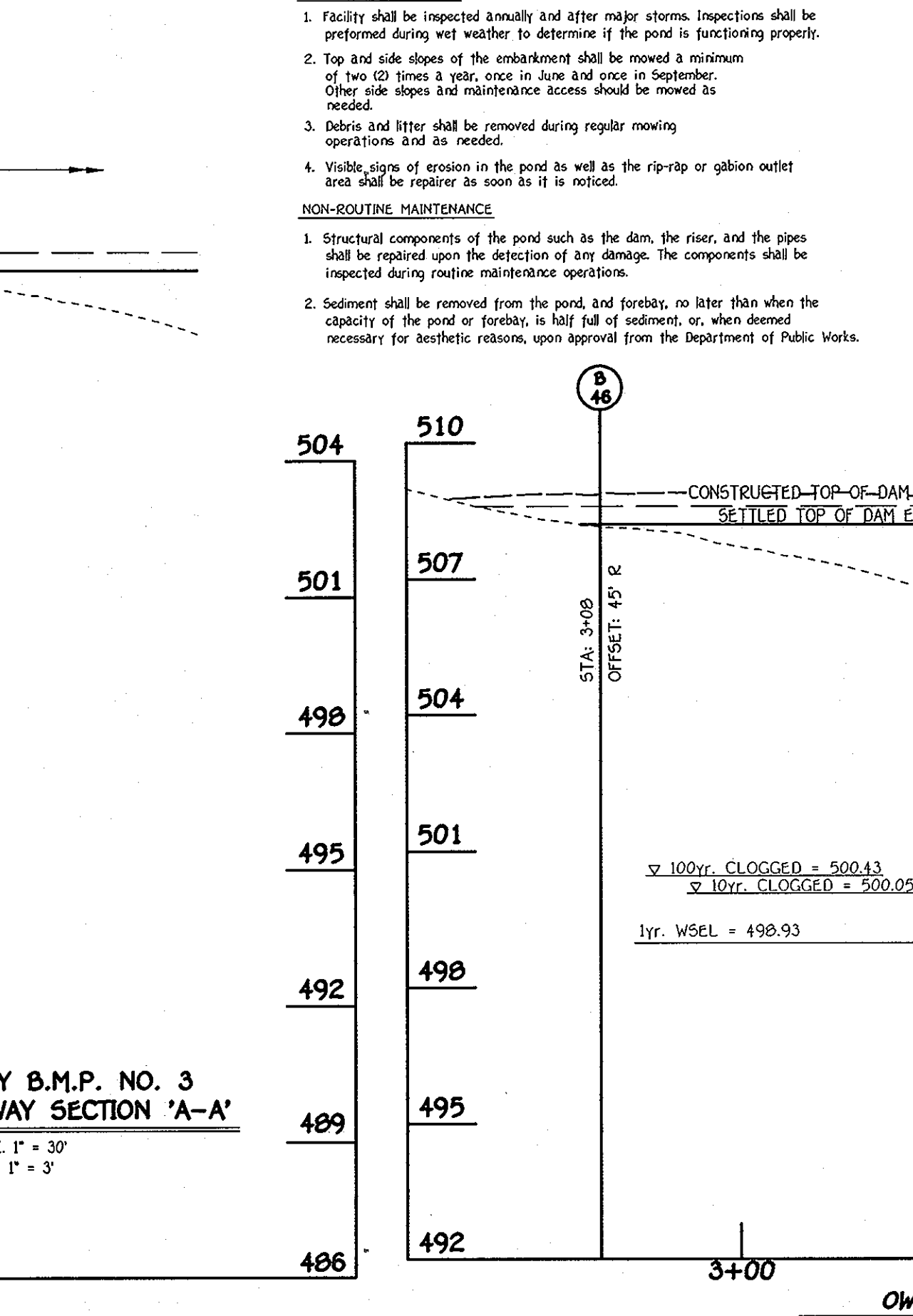
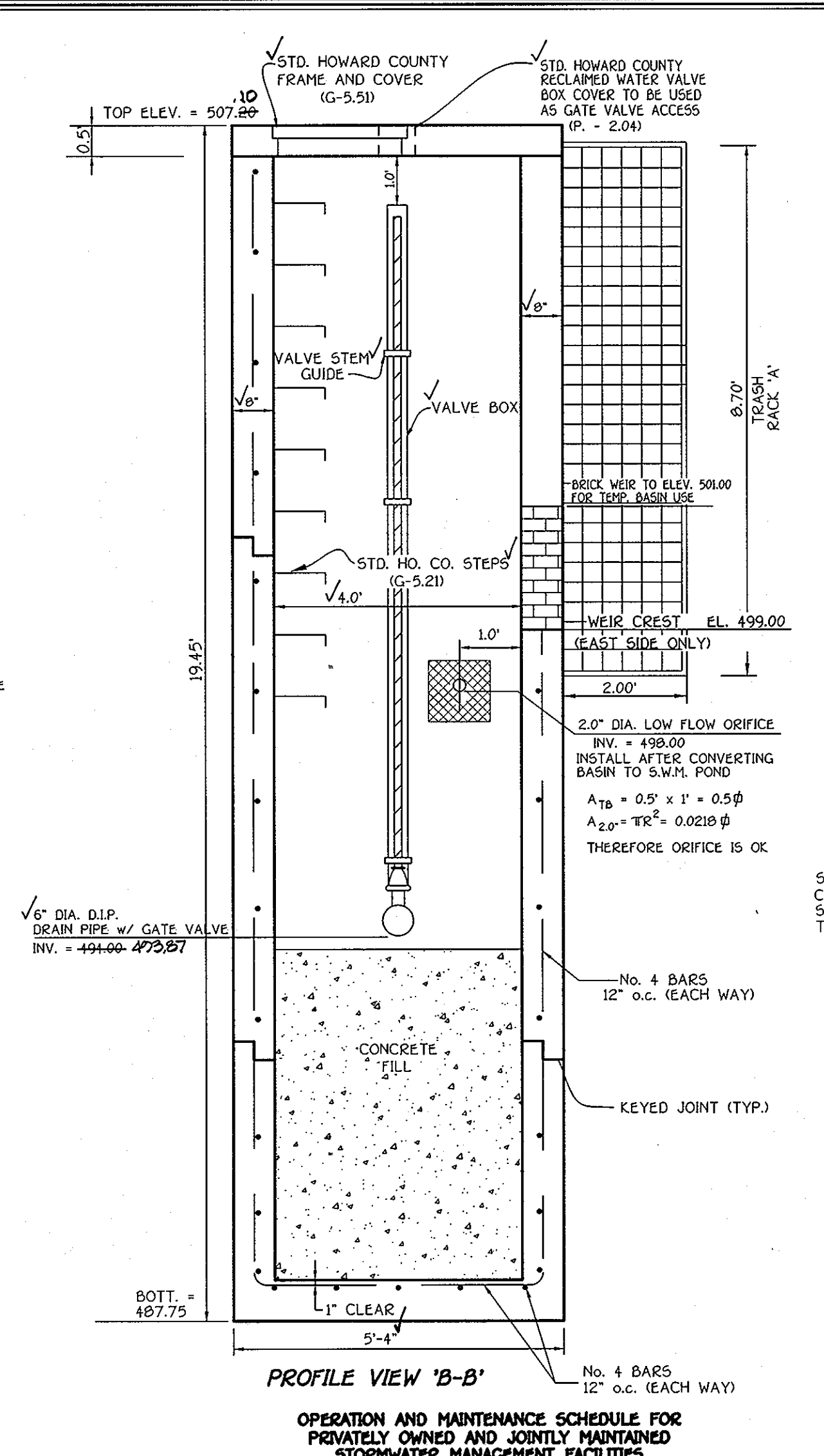
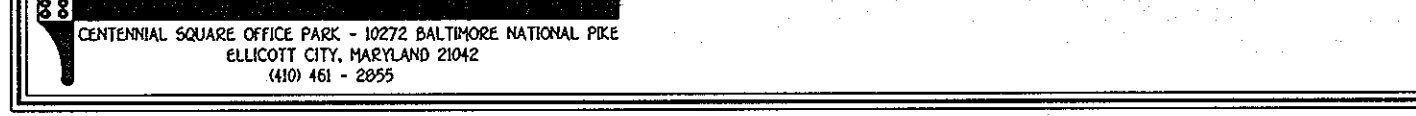
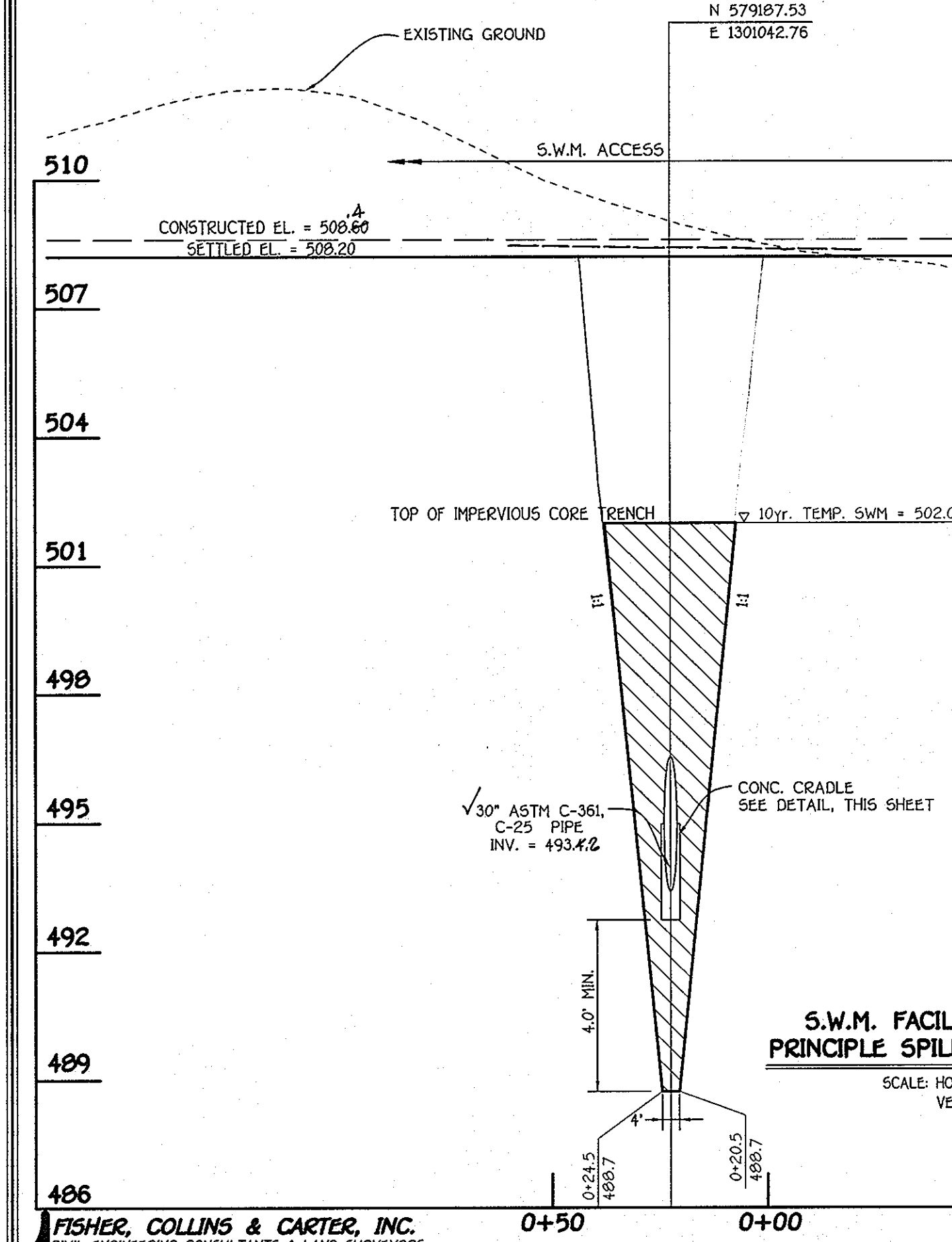
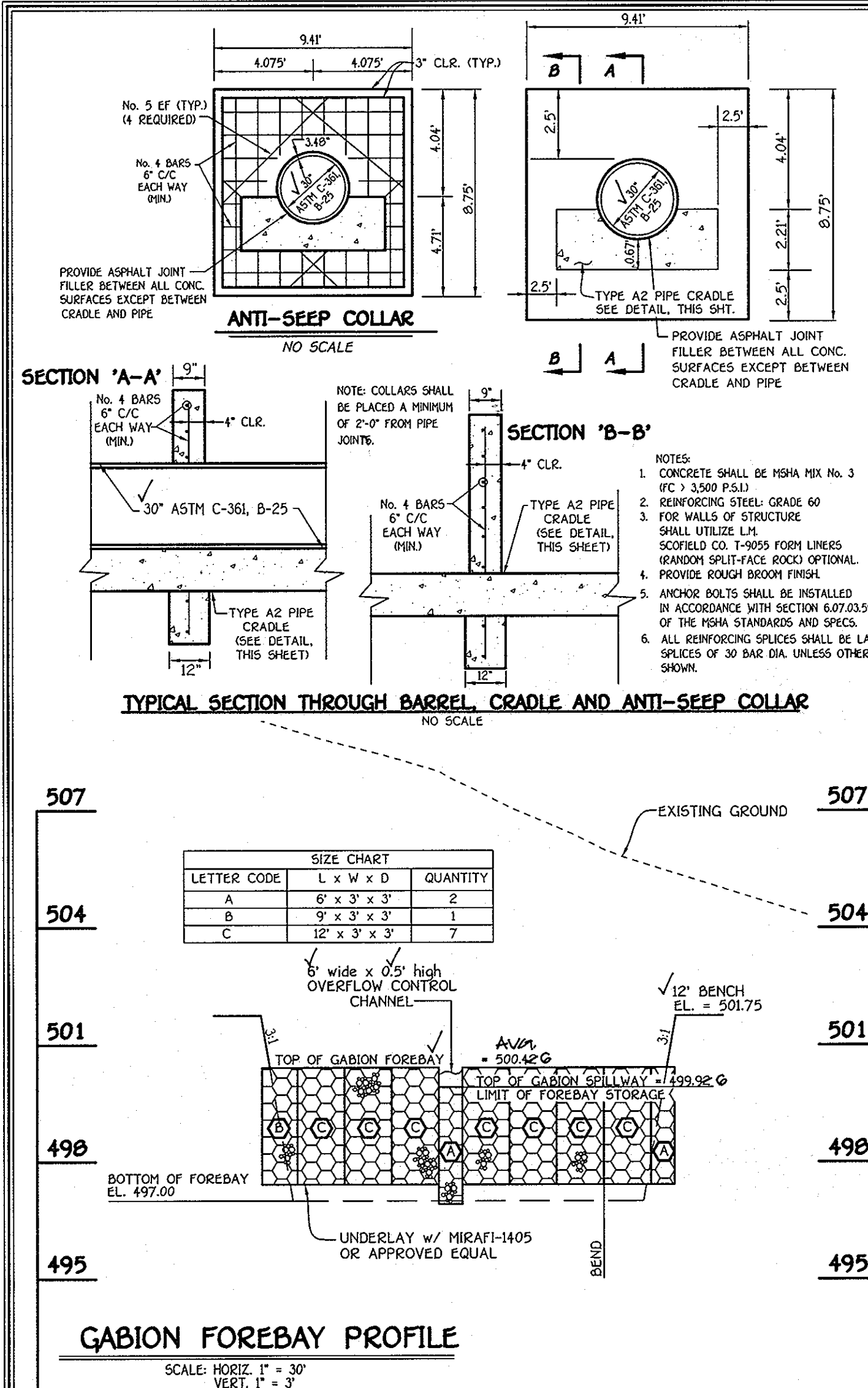
**NON-ROUTINE MAINTENANCE**

- Structural components of the pond such as the dam, the riser, and the spillway shall be repaired upon the detection of any damage. The components shall be inspected during routine maintenance operations.
- Sediment shall be removed from the pond and forebay, no later than when the capacity of the pond or forebay is half full of sediment, or when deemed necessary for aesthetic reasons, upon approval from the Department of Public Works.



**AS BUILT**

DATE: 1-14-09  
TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
DATE: DECEMBER 17, 2008  
SHEET 14 OF 20



By the Developer:  
 I/We Certify That All Development And/Or Construction Will Be Done According To These Plans. And That Any Responsible Personnel Involved In The Construction Project Will Have A Certificate Of Attendance At A Department Of The Environment Approved Training Program For The Control Of Sediment And Erosion Before Beginning The Project. I Shall Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion. I Shall Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion.

Signature of Developer: *Robert Goodwin* Date: 12-23-08  
 Printed Name of Developer: Robert Goodwin

By the Engineer:  
 I Certify That The Design And Construction, Erosion And Sediment Control Represents A Practical And Feasible Plan, Based On My Personal Knowledge Of The Site Conditions. This Plan Was Prepared In Accordance With The Requirements Of The Howard Soil Conservation District. I Have Provided The Developer With My Personal Knowledge Of The Site Conditions. I Shall Engage A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With An "As-Built" Plan Of The Pond Within 30 Days Of Completion.

Signature of Engineer: *A. J. Wallace* Date: 12-23-08  
 Printed Name of Engineer: A. J. Wallace

These Plans For Small Pond Construction Soil Erosion And Sediment Control Meet The Requirements Of The Howard Soil Conservation District.

Approved Department of Public Works: *W. J. Wall* Date: 1-22-09  
 Chief, Bureau of Highways

Approved Department of Planning And Zoning: *C. H. H. H.* Date: 2/5/09  
 Chief, Division Of Land Development

Signature: *W. J. Wall* Date: 2/4/09  
 Chief, Development Engineering Division

AS-BUILT CERTIFICATION  
 I Herby Certify That The Facility Shown On This Plan Was Constructed As Shown On The "As-Built" Plans And Meets The Approved Plans And Specifications.

Signature: *W. J. Wall* Date: 1/28/09  
 Date: 3/17/09

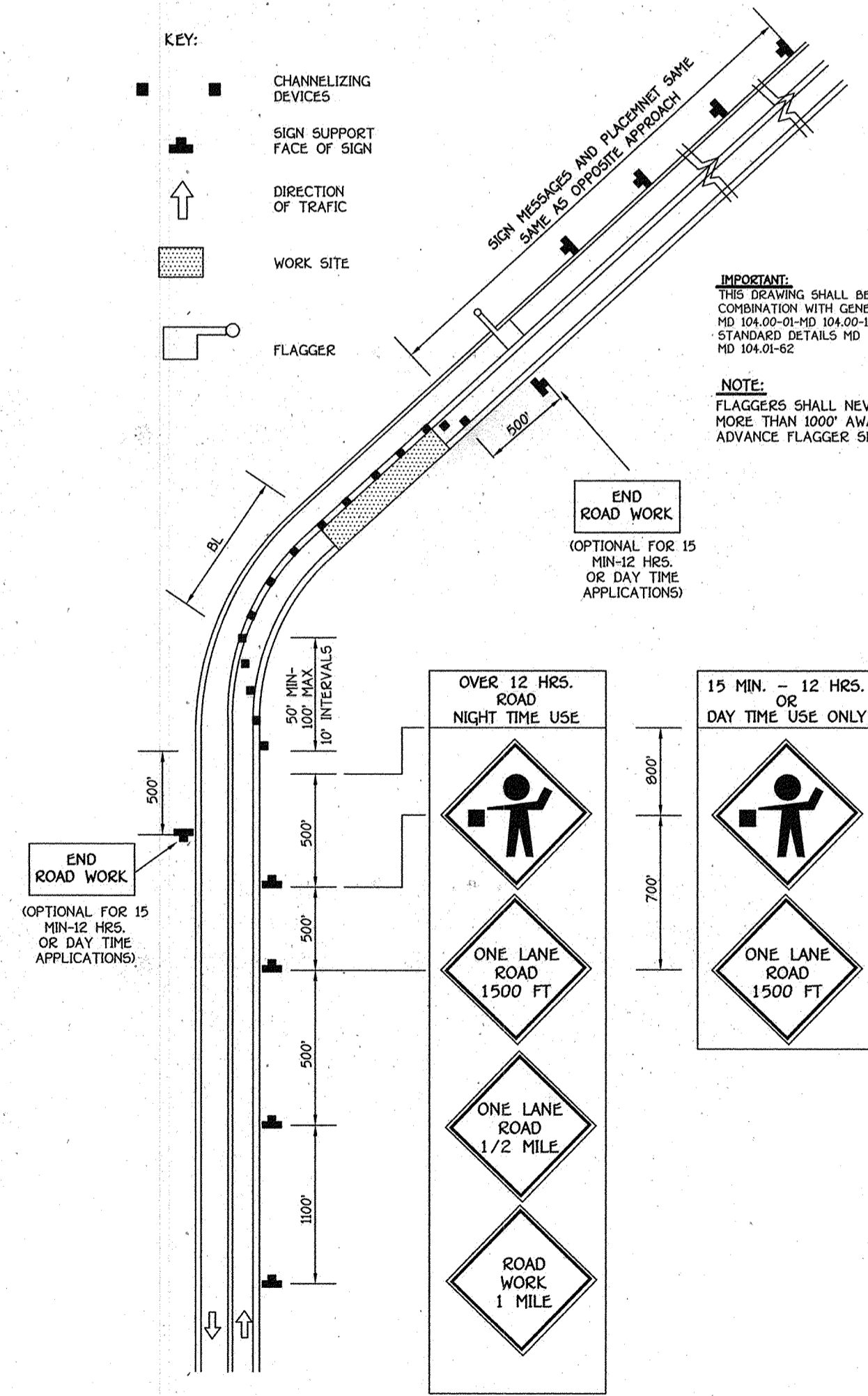
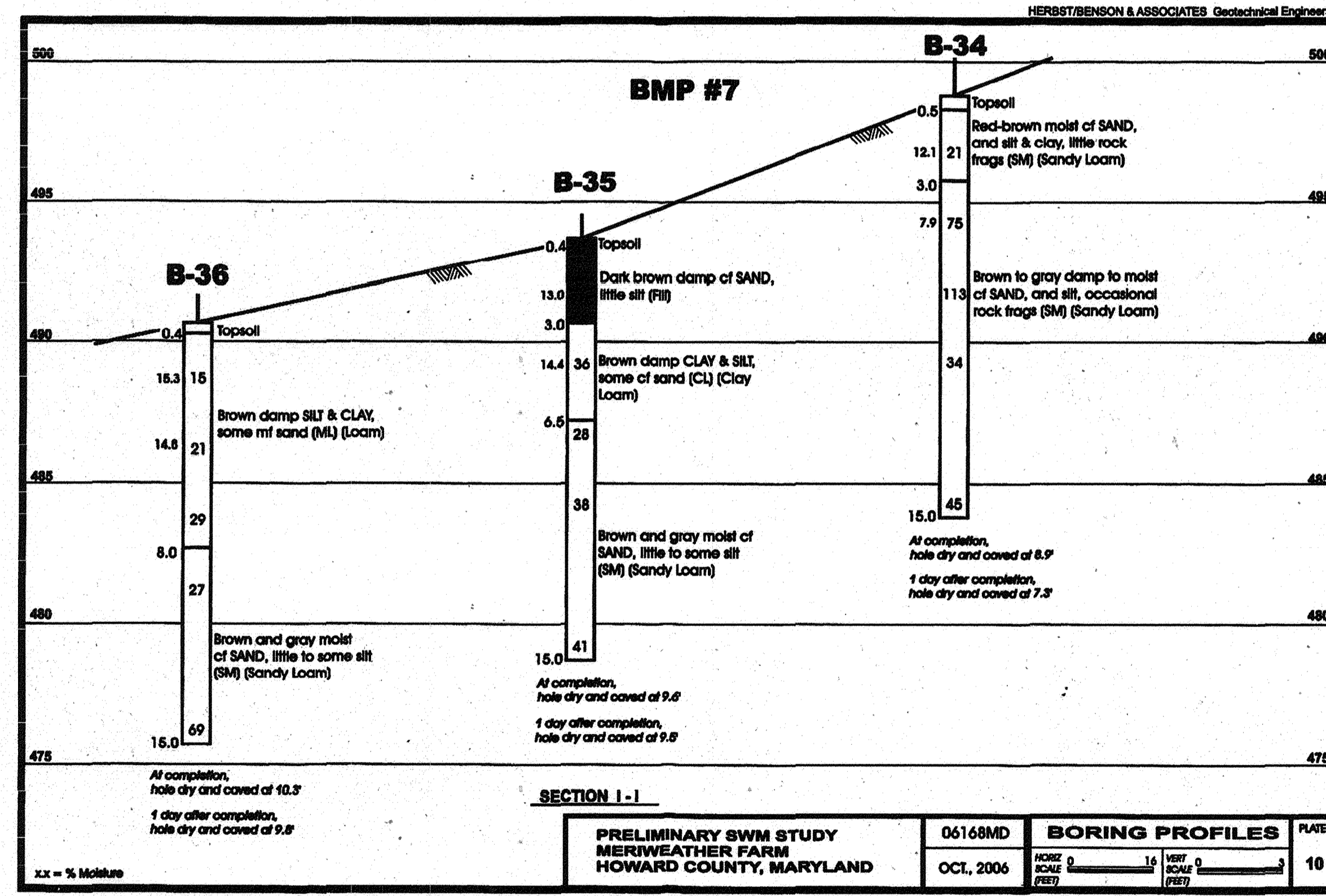
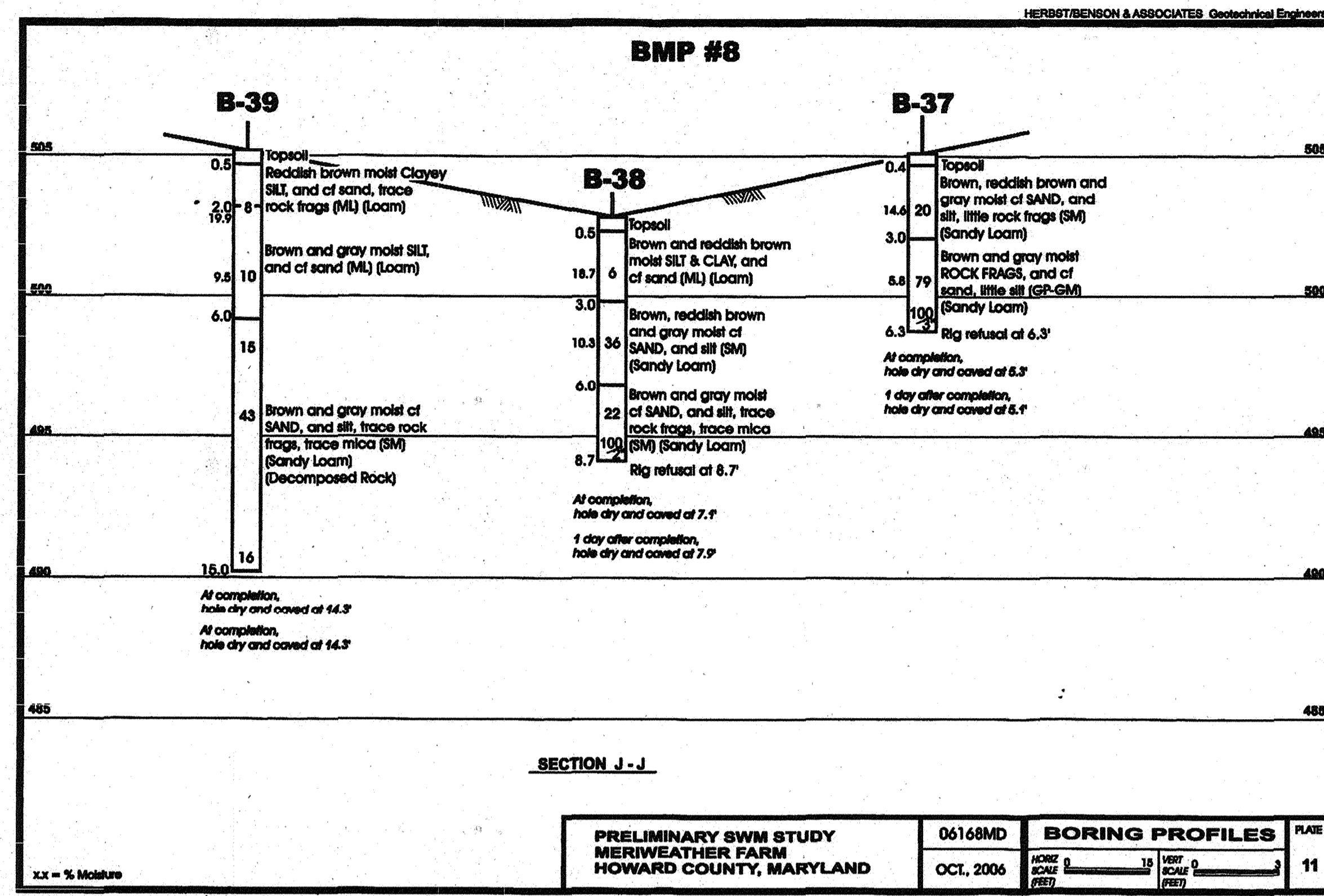
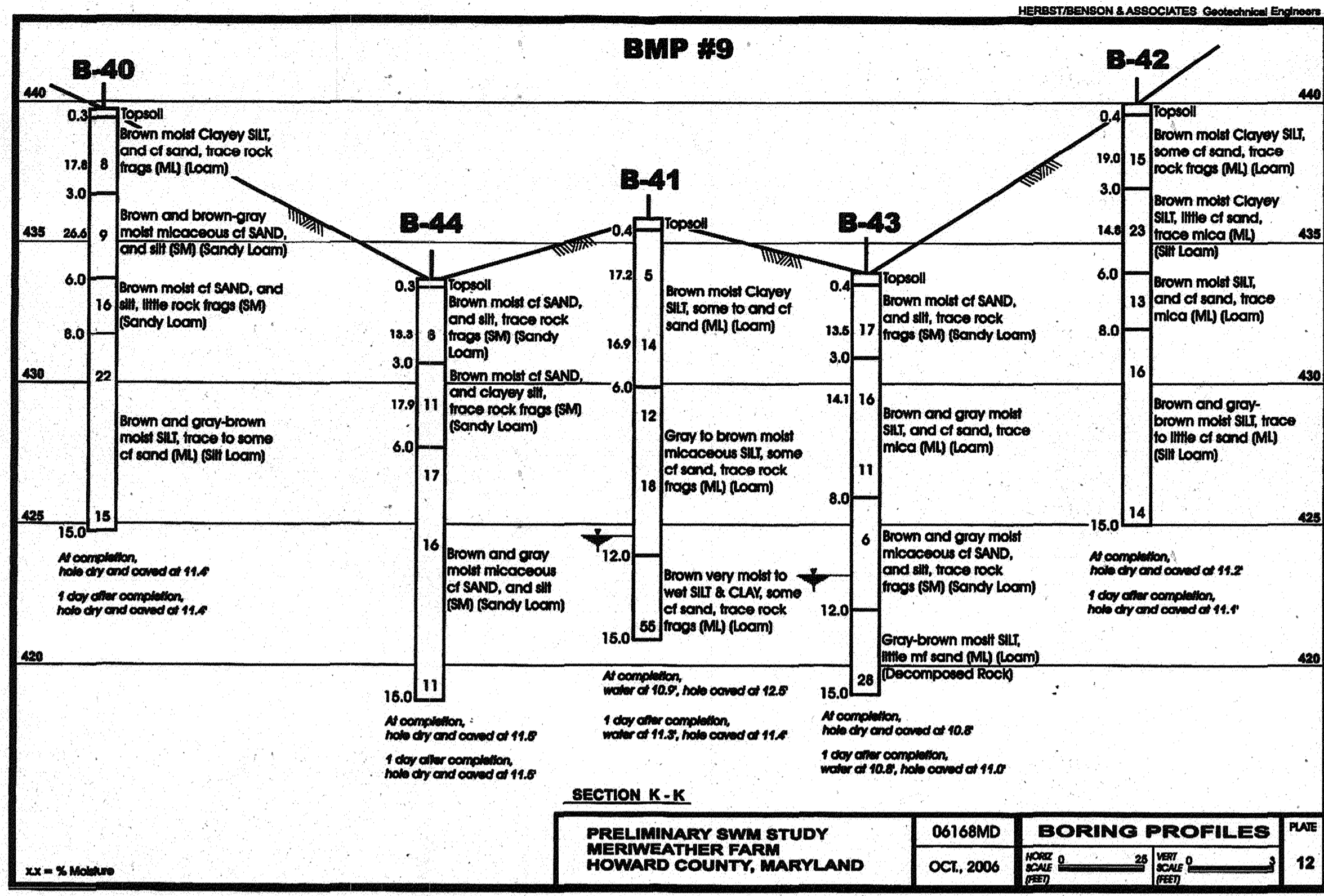
Certify Means To State Or Declare A Professional Opinion Based Upon Onsite Inspections And Material Tests Which Are Conducted During Construction. The Onsite Inspections And Material Tests Are Those Inspections And Tests Deemed Sufficient And Appropriate Commonly Accepted Engineering Standards. Certify Does Not Mean Or Imply A Guarantee By The Engineer Nor Does An Engineer's Certification Relieve Any Other Party From Meeting Requirements Imposed By Contract, Employment, Or Other Means, Including Meeting Commonly Accepted Industry Practices.

NOTE: CONTRACTOR TO ADJUST GRADE AT RISER TO PROVIDE 1.0" MIN. CLR. TO TRASH RACK.

NOTE: CONTRACTOR TO ADJUST GRADE AT RISER TO PROVIDE 1.0" MIN. CLR. TO TRASH RACK.



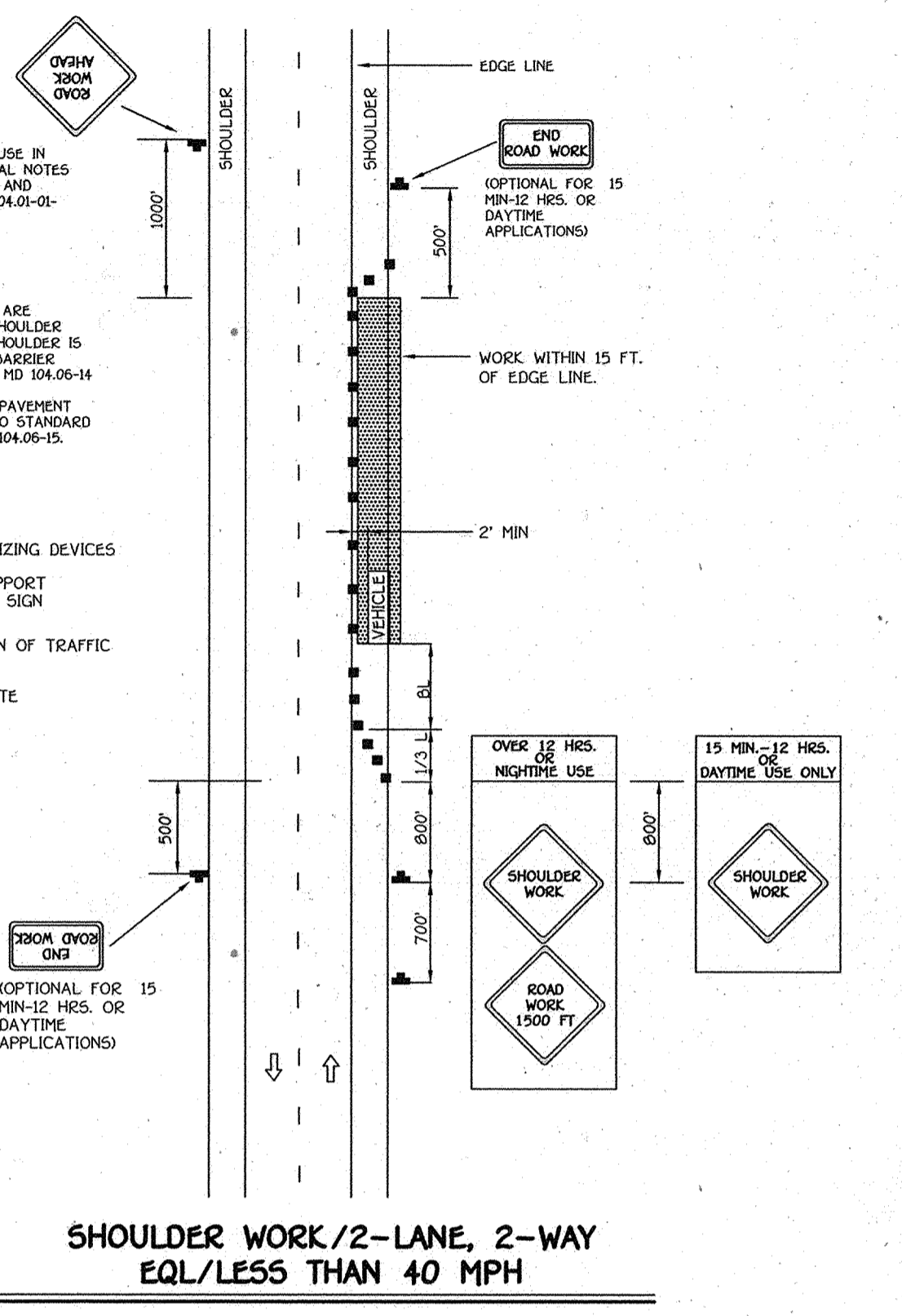




**MAINTENANCE OF TRAFFIC SPECIAL PROVISIONS**

**GENERAL**

- THE PURPOSE OF THIS PORTION OF THE SPECIAL PROVISIONS IS TO SET FOR THE TRAFFIC CONTROL REQUIREMENTS NECESSARY FOR THE SAFE AND EFFICIENT MAINTENANCE TO TRAFFIC WITHIN WORK AREAS, AND TO MINIMIZE ANY INCONVENIENCES TO THE TRAVELING PUBLIC AND THE CONTRACTOR AND/OR PERMITTEE.
- PROPERTY TRAFFIC CONTROL THROUGH WORK AREAS IS ESSENTIAL FOR INSURING THE SAFETY AND THAT OF HIGHWAY WORKERS HAS THE HIGHEST PRIORITY OF ALL TASKS WITHIN THIS PROJECT. THE PROPER APPLICATION OF THE APPROVED TRAFFIC CONTROL PLAN (TCP) WILL PROVIDE THE DESIRED LEVEL OF SAFETY.
- THROUGHOUT THESE SPECIAL PROVISIONS, ANY MENTION OF THE TCP SHALL BE DEEMED TO INCLUDE ANY COMBINATION OF TYPICAL TRAFFIC CONTROL STANDARDS WHICH FORM THE OVERALL TCP FOR THIS PROJECT WHICH HAS BEEN APPROVED BY THE APPROPRIATE SHA TRAFFIC ENGINEER.
- THE CONTRACTOR AND/OR PERMITTEE SHALL BE REQUIRED TO ADHERE TO THE PROVISIONS OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), 1998 EDITION, ESPECIALLY PART VI, AND TO SECTION 814 OF THE MARYLAND DOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS (JANUARY, 1992), INCLUDING ALL REVISIONS AND SUPPLEMENTS TO EACH.
- THE CONTRACTOR AND/OR PERMITTEE SHALL BE REQUIRED TO ADHERE TO THE REQUIREMENTS SET FORTH IN THE TCP AND THESE SPECIAL PROVISIONS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ANY REQUESTS TO MAKE MINOR CHANGES TO THE TCP OR THE SPECIAL PROVISIONS WITH REGARD TO THE TRAFFIC CONTROL ITEMS SHALL BE MADE IN WRITING TO THE ENGINEER A MINIMUM OF THIRTEEN WORKING DAYS PRIOR TO THE PROPOSED SCHEDULING CHANGE. THE CONTRACTOR AND/OR PERMITTEE SHALL HAVE WRITTEN APPROVAL OF THE ENGINEER PRIOR TO THE IMPLEMENTATION OF ANY CHANGE.
- NO WORK SHALL BEGIN ON ANY WORK ACTIVITY OR WORK PHASE UNTIL ALL REQUIRED TRAFFIC CONTROL PATTERNS AND DEVICES INDICATED ON THE TCP FOR THAT ACTIVITY OR PHASE ARE COMPLETELY AND CORRECTLY IN PLACE TO HAVE BEEN CHECKED FOR APPROVED USAGE.
- GENERAL AND SPECIFIC WARNING SIGNS SHALL ONLY BE IN PLACE WHEN SPECIFIC WORK TASKS AND ACTIVITIES ARE ACTUALLY UNDERWAY OR CONDITIONS EXIST THAT POSE A POTENTIAL HAZARD TO THE PUBLIC, AND ANY ADDITIONAL SIGNING HAS BEEN APPROVED BY THE APPROPRIATE SHA TRAFFIC ENGINEER. NOTE: THE PRACTICE OF PLACING SIGNING AND OTHER TRAFFIC CONTROL DEVICES IN ADDITION TO THOSE INDICATED ON THE APPROVED TCP IS NOT PERMITTED.
- THE CONTRACTOR AND/OR PERMITTEE SHALL PROVIDE, MAINTAIN IN NEW CONDITION, AND MOVE WHEN NECESSARY, OR AS DIRECTED BY THE ENGINEER, ALL TRAFFIC CONTROL DEVICES USED FOR THE GUIDANCE AND PROTECTION OF MOTORISTS, PEDESTRIANS, AND WORKERS.
- ALL TRAFFIC CONTROL DEVICES REQUIRED BY THE TCP SHALL BE KEPT IN GOOD CONDITION, FULLY PERFORMING AS SET FORTH IN THE TCP, THE MUTCD AND/OR SECTION 814 OF THE SPECIFICATIONS. FOR REFLECTIVE DEVICES, A PARTICULAR DEVICE IS ASSUMED TO HAVE FAILED TO MEET MINIMUM OPERATIONAL STANDARDS WHEN THE DEVICE NO LONGER HAS RETRO-REFLECTANCE CAPABILITY OF AT LEAST 60% OF THE SPECIFIED MINIMUM VALUE OVER AT LEAST 90% OF THE VISIBLE REFLECTIVE SURFACE.
- ALL TRAFFIC CONTROL DEVICES NOT REQUIRED FOR THE SAFE CONDUCT OF TRAFFIC SHALL BE PROMPTLY REMOVED, COMPLETELY COVERED, TURNED AWAY FROM TRAFFIC, OR OTHERWISE TAKEN OUT OF SERVICE. IT IS INTENDED THAT NO TRAFFIC CONTROL DEVICE IS TO BE IN SERVICE WHEN THERE IS NO CLEAR CUT REASON FOR THE DEVICE.
- THROUGHOUT THE PERIODS OF WORK ACTIVITIES, TRAFFIC SHALL BE MAINTAINED BY IMPLEMENTING THE APPROVED TCP. IN LIEU OF THE TCP PREPARED FOR THIS PROJECT, AND/OR INDIVIDUAL TYPICAL TRAFFIC CONTROL STANDARDS, THE CONTRACTOR AND/OR PERMITTEE HAS THE OPTION OF PREPARING AND SUBMITTING A TCP, WHOLLY OR IN PART, OF HIS OWN DESIGN FOLLOWING GUIDELINES SET FORTH IN THE MUTCD AND PRESCRIBED BY THE ADMINISTRATION. A TCP DEVELOPED BY THE CONTRACTOR AND/OR PERMITTEE SHALL NOT BE IMPLEMENTED UNTIL ADVANCE WRITTEN APPROVAL IS OBTAINED FROM THE ENGINEER. TCP'S MAY BE IMPLEMENTED WITHIN A SINGLE PROJECT OR JOINTLY BETWEEN TWO OR MORE PROJECTS. IN SITUATIONS WHERE TCP'S JOINTLY IMPLEMENTED, CARE SHALL BE EXERCISED TO PRESENT CORRECT AND NON-CONFLICTING GUIDANCE TO THE TRAVELING PUBLIC.
- THROUGHOUT THESE SPECIAL PROVISIONS, WHERE SPEED OF TRAFFIC IS NOTED, THIS MEANS THE POSTED SPEED OR PREVAILING TRAVEL SPEED, WHICHEVER IS HIGHER, UNLESS OTHERWISE NOTED.
- TRAFFIC SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT THE ENTIRE LENGTH OF THE PROJECT, UNLESS OTHERWISE NOTED. NO TRAVEL LANES OTHER THAN THOSE DESIGNATED FOR POSSIBLE CLOSURE IN THE TCP SHALL BE CLOSED WITHOUT OBTAINING PRIOR APPROVAL FROM THE ENGINEER. ALL INGRESS AND EGRESS TO THE WORK AREA BY THE CONTRACTOR AND/OR PERMITTEE SHALL BE PERFORMED WITH THE FLOW OF TRAFFIC.



**FISHER, COLLINS & CARTER, INC.**  
CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
CONTINENTAL SQUARE OFFICE PARK - 10272 BALDWIN NATIONAL PARK  
ELLICOTT CITY, MARYLAND 21114  
410-481-2995

**OWNER**  
FRANCES B. DEVLIN  
43 LITCHFIELD ROAD  
LITCHFIELD, CT. 06759  
(410) 489-7900

**DEVELOPER**  
HERITAGE LAND DEVELOPMENT  
15950 NORTH AVENUE, P.O. BOX 482  
LISBON, MARYLAND 21765  
(410) 489-7900

**STATE OF MARYLAND**  
ALDO R. VITELLO, P.E.  
Professional Engineer  
I hereby certify that these documents were prepared by me, and that I am a duly Licensed Professional Engineer under the laws of the State of Maryland, License No. 20726, Expiration Date 2-22-09.

**SOIL BORINGS & TEMPORARY TRAFFIC CONTROL**  
**MERIWETHER FARM**  
**SECTION ONE**  
LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A';  
NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
AND NON-BUILDABLE PARCEL 'E'  
ZONED: RC-DEO  
TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
DATE: DECEMBER 17, 2008  
SHEET 17 OF 20

Approved Department of Public Works  
 Chief, Bureau of Highways  
 Date: 1-22-09

Approved Department of Planning and Zoning  
 Chief, Division of Land Development  
 Date: 2/5/09

Approved  
 Chief, Development Engineering Division  
 Date: 2/14/09

DRAINAGE AREA DATA				
STRUCTURE NO.	DRAINAGE AREA	AREA	C	ZONED
I-1	A	0.16 AC.	0.32	RC-DEO
I-1A	A-1	0.23 AC.	0.39	RC-DEO
I-2	B	2.16 AC.	0.39	RC-DEO
I-3	C	1.08 AC.	0.41	RC-DEO
I-4	D	0.22 AC.	0.61	RC-DEO
I-5	E	0.22 AC.	0.78	RC-DEO
I-6	F	0.76 AC.	0.39	RC-DEO
I-7	G	0.40 AC.	0.81	RC-DEO
I-8	H	0.99 AC.	0.57	RC-DEO
I-9	I	0.04 AC.	0.78	RC-DEO
I-10	J	0.54 AC.	0.43	RC-DEO
I-11	K	0.09 AC.	0.80	RC-DEO
I-12	L	0.10 AC.	0.80	RC-DEO
I-13	M	0.36 AC.	0.55	RC-DEO
I-14	N	0.64 AC.	0.57	RC-DEO
I-15	O	0.09 AC.	0.61	RC-DEO
I-16	P	0.10 AC.	0.50	RC-DEO



Note: This Plan is For Drainage Area Use Only.

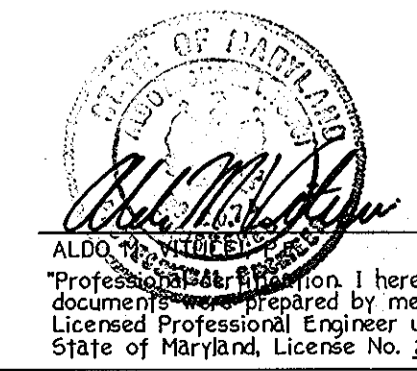
PLAN  
 SCALE: 1" = 100'

FISHER, COLLINS & CARTER, INC.  
 CIVIL ENGINEERING CONSULTANTS & LAND SURVEYORS  
 CENTRAL SQUARE OFFICE PARK - 1072 BALTIMORE NATIONAL PRX  
 ELLICOTT CITY, MARYLAND 21042  
 4100 461 - 2995

No.	Description	Date
1	Removed Private Sewer Device For Parcel A'	9/18/12
2	Removed Underground Fire Suppression Tank / Roll-off Area	4/12/11

**OWNER**  
 FRANCES B. DEVLIN  
 43 LITCHFIELD ROAD  
 LITCHFIELD, CT. 06759  
 (410) 499-7900

**DEVELOPER**  
 HERITAGE LAND DEVELOPMENT  
 15950 NORTH AVENUE, P.O. BOX 482  
 LISBON, MARYLAND 21765  
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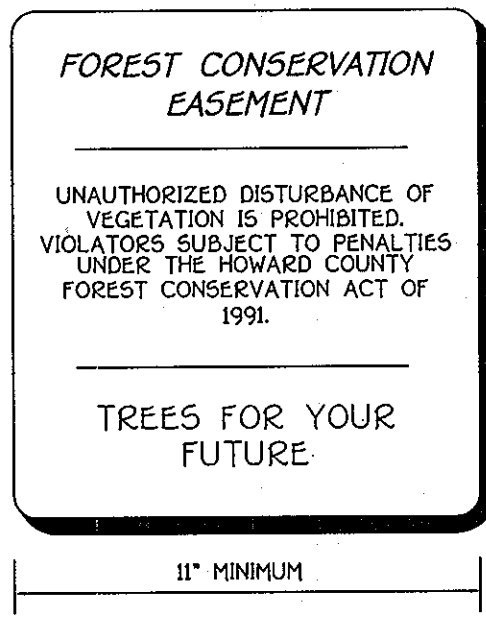
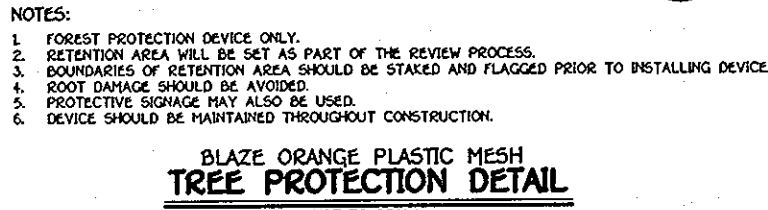
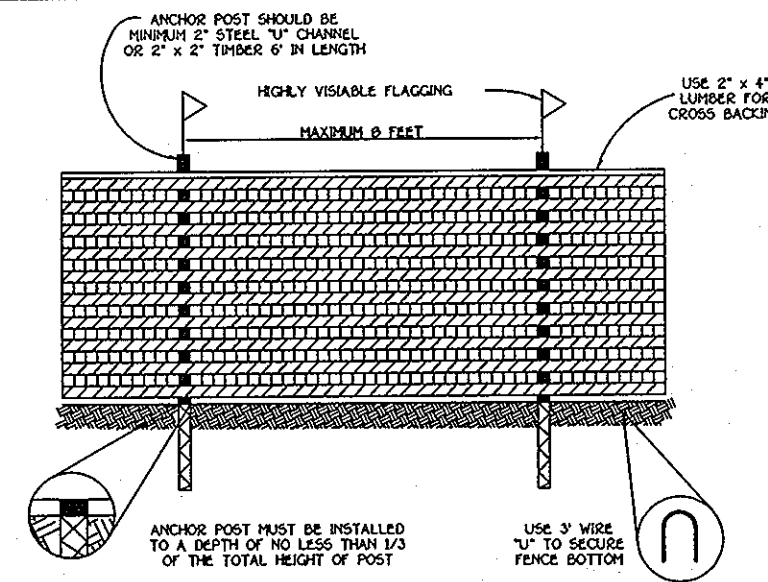
DATE: 12-22-08  
 I, ALDO J. MARIANO, hereby certify that these documents were prepared by me, and that I am a duly Licensed Professional Engineer under the laws of the State of Maryland, License No. 20748, Expiration Date 2-22-09.

STORM DRAIN DRAINAGE AREA MAP  
**MERIWETHER FARM**  
 SECTION ONE  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'

TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 18 OF 20

**FCP NOTES:**

- Any Forest Conservation Easement (FCE) area shown hereon is subject to protective covenants which may be found in the Land Records of Howard County which restrict the disturbance and use of these areas.
- Forested areas occurring outside of the FCE shall not be considered part of the FCE and shall not be subject to protective land covenants.
- Limits of disturbance shall be restricted to areas outside the limit of temporary fencing or the FCE boundary, whichever is greater.
- There shall be no clearing, grading, construction or disturbance of vegetation in the Forest Conservation Easement, except as permitted by Howard County DPZ.
- No stockpiles, parking areas, equipment clearing areas, etc. shall occur within areas designated as Forest Conservation Easements.
- Temporary fencing shall be used to protect forest resources during construction. The fencing shall be placed along all FCE boundaries which occur within 15 feet of the proposed limits of disturbance.
- Permanent signage shall be placed 50-100' apart along the boundaries of all areas included in Forest Conservation Easements.
- The Forest Conservation Act requirements for this project include 18.6 acres of forest retention. The forestation obligation will be met by providing a total retention area of 18.6 acres.



Approved: Department of Public Works  
 Chief: Bureau of Highways  
 Date: 1-22-09

Approved: Department of Planning and Zoning  
 Chief: Division of Land Development  
 Date: 2/5/09

Approved: Development Engineering Division  
 Chief: Development Engineering Division  
 Date: 2/19/09

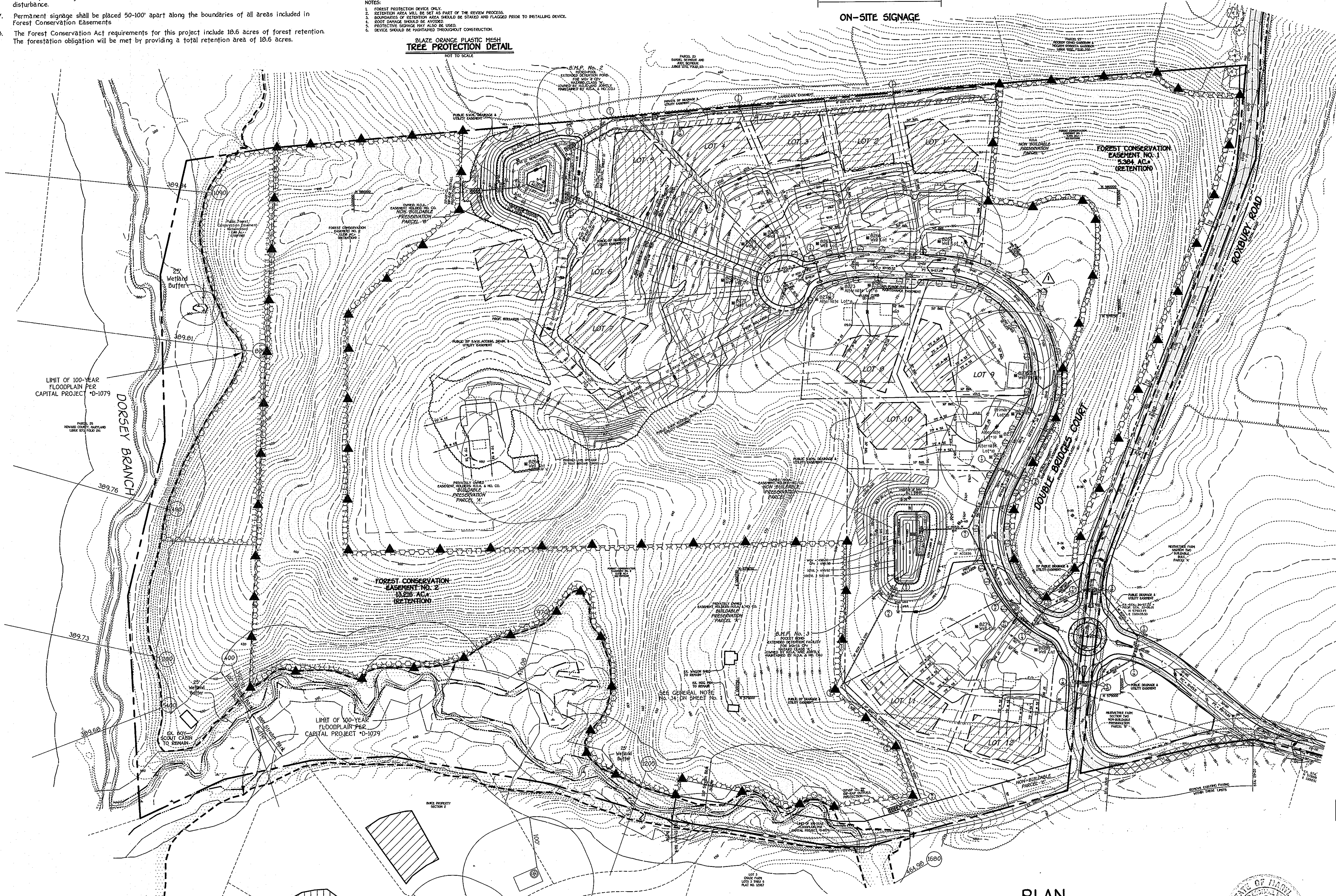
**FOREST CONSERVATION WORKSHEET**

NET TRACT AREA	ACRES
A. TOTAL TRACT AREA	60.88
B. DEDUCTIONS (FLOODPLAIN)	6.92
C. NET TRACT AREA = TOTAL TRACT (A) - DEDUCTIONS (B)	54.06
LAND USE CATEGORY: MEDIUM DENSITY RESIDENTIAL	
D. AFFORESTATION THRESHOLD (NET TRACT AREA (C) x 20%)	10.8
E. CONSERVATION THRESHOLD (NET TRACT AREA (C) x 25%)	13.5
EXISTING FOREST COVER	
F. EXISTING FOREST COVER WITHIN THE NET TRACT AREA	38.9
G. AREA OF FOREST ABOVE CONSERVATION THRESHOLD	25.4
IF THE EXISTING FOREST COVER (F) IS GREATER THAN THE CONSERVATION THRESHOLD (E), THEN G = F - E, OTHERWISE G = 0.	
BREAK-EVEN POINT	
H. BREAK-EVEN POINT (AMOUNT OF FOREST THAT MUST BE RETAINED SO THAT NO MITIGATION IS REQUIRED)	10.6
(I) IF THE AREA OF FOREST ABOVE CONSERVATION THRESHOLD (G) IS GREATER THAN 0, THEN H = (0.2 x THE AREA OF FOREST ABOVE CONSERVATION THRESHOLD (G)) + THE CONSERVATION THRESHOLD (E).	
(J) IF THE AREA OF FOREST ABOVE CONSERVATION THRESHOLD (G) IS EQUAL TO 0, THEN H = EXISTING FOREST COVER (F).	
I. FOREST CLEARING PERMITTED WITHOUT MITIGATION	20.3
J = EXISTING FOREST COVER (F) - BREAK-EVEN POINT (H)	
PROPOSED FOREST CLEARING	
J. TOTAL AREA OF FOREST TO BE CLEARED	16.07
K. TOTAL AREA OF FOREST TO BE RETAINED	18.60
L = EXISTING FOREST COVER (F) - FOREST TO BE CLEARED (J)	
PLANTING REQUIREMENTS	
IF THE TOTAL AREA OF FOREST TO BE RETAINED (K) IS AT OR ABOVE THE BREAK-EVEN POINT (H), NO PLANTING IS REQUIRED, AND NO FURTHER CALCULATIONS ARE NECESSARY (L=0, M=0, N=0, P=0, Q=0, R=0).	
OTHERWISE, CALCULATE THE PLANTING REQUIREMENTS AS FOLLOWS:	
L. REFORESTATION FOR CLEARING ABOVE THE CONSERVATION THRESHOLD	
(1) IF THE TOTAL AREA OF FOREST TO BE RETAINED (K) IS GREATER THAN THE CONSERVATION THRESHOLD (E), THEN L = THE AREA OF FOREST TO BE CLEARED (J) x 0.25.	
(2) IF THE FOREST TO BE RETAINED (K) IS LESS THAN OR EQUAL TO THE CONSERVATION THRESHOLD (E), THEN L = AREA OF FOREST ABOVE CONSERVATION THRESHOLD (G) x 0.25	
M. REFORESTATION FOR CLEARING BELOW THE CONSERVATION THRESHOLD	0
(1) IF EXISTING FOREST COVER (F) IS GREATER THAN THE CONSERVATION THRESHOLD (E) AND THE FOREST TO BE RETAINED (K) IS LESS THAN OR EQUAL TO THE CONSERVATION THRESHOLD (E), THEN M = 2.0 x (CONSERVATION THRESHOLD (E) - FOREST TO BE RETAINED (K))	
(2) IF EXISTING FOREST COVER (F) IS LESS THAN OR EQUAL TO THE CONSERVATION THRESHOLD (E), THEN M = 2.0 x FOREST TO BE CLEARED (J)	
N. CREDIT FOR RETENTION ABOVE THE CONSERVATION THRESHOLD	0
IF THE AREA OF FOREST TO BE RETAINED (K) IS GREATER THAN THE CONSERVATION THRESHOLD (E), THEN N = K - E, OTHERWISE N = 0	
P. TOTAL REFORESTATION REQUIRED P = L + M - N	0
Q. TOTAL AFFORESTATION REQUIRED	0
IF EXISTING FOREST COVER (F) IS LESS THAN THE AFFORESTATION THRESHOLD (D), THEN	
Q = AFFORESTATION THRESHOLD (D) - EXISTING FOREST COVER (F)	0
R. TOTAL PLANTING REQUIREMENT R = P + Q	0

No.	Description	Date
1	Removed Private Swm Device For Parcel 'A'	9/13/12
2	Removed Underground Fire Suppression Tank & Full-off Area	4/12/11

Revisions

Note: This Plan is For Forest Conservation Use Only.



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**Eco-Science Professionals, Inc.**  
 CONSULTING ECOLOGISTS

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 Certification # WD093MD08100448  
 12/19/08  
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**PLAN**  
 SCALE: 1" = 100'

STATE OF MARYLAND  
 PROFESSIONAL ENGINEER  
 License No. 20718, Expiration Date 2-22-09

122209  
 DATE

**FOREST CONSERVATION PLAN  
 MERIWETHER FARM  
 SECTION ONE**  
 LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
 NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
 AND NON-BUILDABLE PARCEL 'E'  
 ZONED: RC-DEO  
 TAX MAP No. 21 GRID No. 14, 15, 20 & 21 PARCEL No. 24  
 FOURTH ELECTION DISTRICT HOWARD COUNTY, MARYLAND  
 DATE: DECEMBER 17, 2008  
 SHEET 19 OF 20

**Infiltration and Filter System Construction Specifications**

Infiltration and filter systems either take advantage of existing permeable soils or create a permeable medium such as sand for VC, and Re v. In most instances where permeability is great, these facilities may be used for QP as well. The most common systems include infiltration trenches, infiltration basins, sand filters, and organic filters.

When properly planned, vegetation will thrive and enhance the functioning of these systems. For example, pre-treatment buffers will trap sediments that often are bound with phosphorus and metals. Vegetation planted in the facility will aid in nutrient uptake and water storage. Additionally, plant roots will provide arteries for stormwater to permeate soil to groundwater recharge. Finally, successful plantings provide aesthetic value and wildlife habitat making these facilities more desirable to the public.

**Design Constraints:**

- > Planting buffer strips of at least 20 feet will cause sediments to settle out before reaching the facility, thereby reducing the possibility of clogging.
- > Determine areas that will be saturated with water and water table depth so that appropriate plants may be selected (hydrology will be similar to bioretention facilities, see figure A.5 and Table A.4 for planting material guidance).
- > Plants known to send down deep taproots should be avoided in systems where filter fabric is used as part of facility design.
- > Test soil conditions to determine if soil amendments are necessary.
- > Plants shall be located so that access is possible for structure maintenance.
- > Stabilize heavy flow areas with erosion control matting or sod.
- > Temporarily divert flows from seeded areas until vegetation is established.
- > See Table A.5 for additional design considerations.

**Bio-retention**

**Soil Bed Characteristics**

The characteristics of the soil for the bioretention facility are perhaps as important as the facility location, size, and treatment volume. The soil must be permeable enough to allow runoff to filter through the media, which having characteristics suitable to promote and sustain a robust vegetative cover. In addition, much of the nutrient pollutant uptake (nitrogen and phosphorus) is accomplished through absorption and microbial activity within the soil profile. Therefore, soils must balance their chemical and physical properties to support biotic communities above and below ground.

The planting soil should be a sandy loam, loamy sand, loam (USDA), or a loam/sand mix (should contain a minimum 35 to 60% sand, by volume). The clay content for these soils should be less than 25% by volume. Environmental Quality Resources (EQR), 1996; Engineering Technology Inc. and Biohabitats, Inc. (ETAB), 1993. Soils should fall within the SM, ML, S, classifications of the Unified Soil Classification System (USCS). A permeability of at least 1.0 inch per day (0.577 in) is required (a conservative value of 0.5 feet per day is used for design). The soil should be free of stumps, roots, or other woody material over 1" in diameter. Brush or seeds from noxious weeds (e.g., Johnson Grass, Mugwort, Nutsedge, and Canada Thistle or other noxious weeds as specified under COMAR 15.08.01.05.) should not be present in the soil.

Placement of the planting soil should be in 12 to 18 lifts that are loosely compacted (tamped lightly with a hand bucket or traversed by dozer tracks). The specific characteristics are presented in Table B.3.

Table B.3 Planting Soil Characteristics

Parameter	Value
pH range	5.2 to 7.00
Organic matter	1.5 to 4.0% (by weight)
Magnesium	35 lbs. per acre, minimum
Phosphorus (phosphate - P2O5)	75 lbs. per acre, minimum
Potassium (potash - K2O)	85 lbs. per acre, minimum
Soluble salts	500 ppm
Clay	10 to 25 %
Silt	30 to 55 %
Sand	35 to 60 %

**Mulch Layer**

The mulch layer plays an important role in the performance of the bioretention system. The mulch layer helps maintain soil moisture and avoids surface sealing, which reduces permeability. Mulch helps prevent erosion, and provides a microenvironment suitable for soil biota at the mulch/soil interface. It also serves as a pretreatment layer, trapping the finer sediments, which remain suspended after the primary pretreatment.

The mulch layer should be standard landscape style single or double shredded hardwood mulch or chips. The mulch layer should be well aged (aged and stored for at least 12 months) uniform in color, and free of other materials such as wood seeds, soil, roots, etc. The mulch should be applied to a maximum depth of three inches. Grass clippings should not be used as a mulch material.

**Planting Guidance**

Plant material selection should be based on the goal of simulating a terrestrial forested community of native species. Bioretention simulates an upland-species ecosystem. The community should be dominated by trees, but have a distinct community of understory trees, shrubs and herbaceous materials by creating a diverse, dense plant cover, a bioretention facility will be able to treat stormwater runoff and withstand urban stresses from insects, disease, drought, temperature, wind, and exposure.

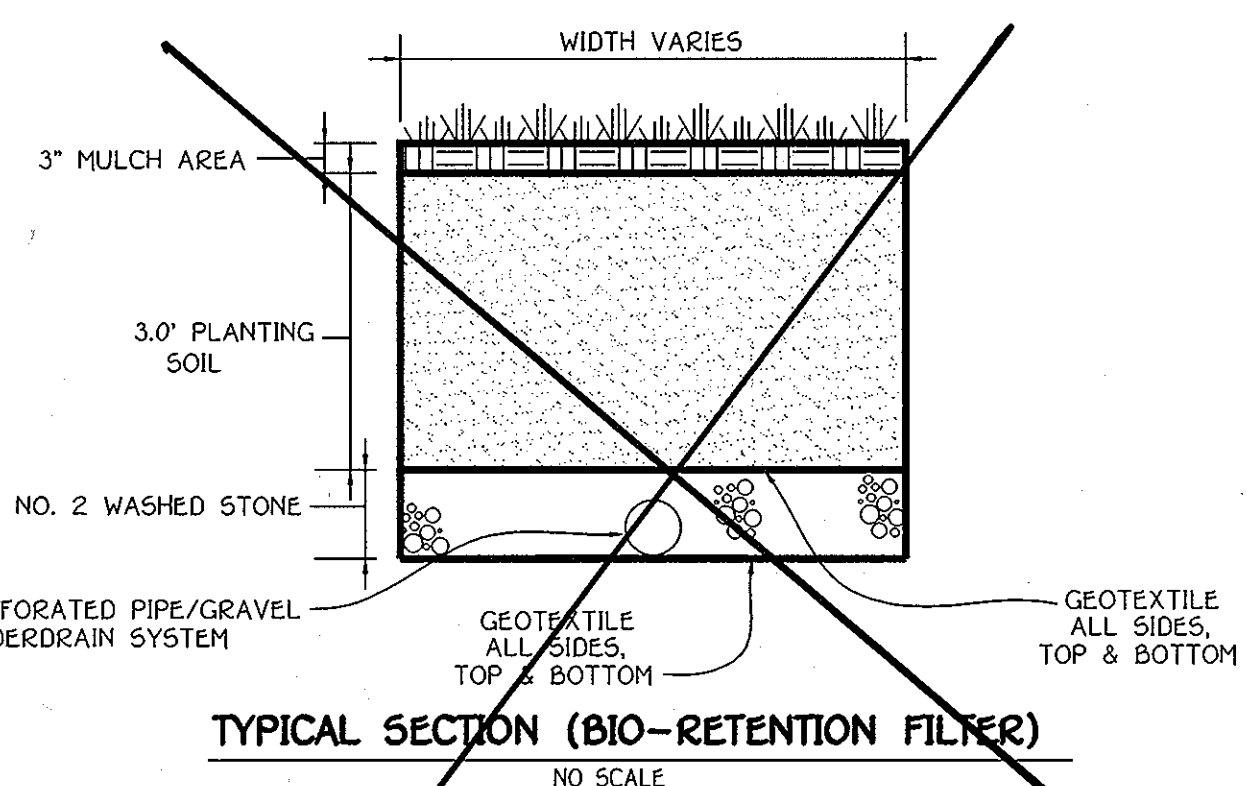
The proper selection and installation of plant materials is key to a successful system. There are essentially three zones within a bioretention facility (figure A.5). The lowest elevation supports plant species adapted to standing and fluctuating water levels. The middle elevation supports plants that tolerate drier soil conditions, but can still tolerate occasional inundation by water. The outer edge is the highest elevation and generally supports plants adapted to drier conditions. A sample of appropriate plant materials for bioretention facilities are included in Table A.4. The layout of plant material should be flexible, but should follow the general principals described in Table A.5. The objectives is to have a system, which resembles a random, and natural plant growth, while maintaining for plant establishment and growth. For a more extensive bioretention plan, consult ETAB, 1993 or Clayton and Schueler, 1997.

**PRIVATE BIO-RETENTION FILTER OPERATION & MAINTENANCE SCHEDULE**

- ANNUAL MAINTENANCE OF PLANT MATERIAL, MULCH LAYER AND SOIL LAYER IS REQUIRED. MAINTENANCE OF MULCH AND SOIL IS LIMITED TO CORRECTING AREAS OF EROSION OR WASH OUT. ANY MULCH REPLACEMENT SHALL BE DONE IN THE SPRING. PLANT MATERIAL SHALL BE REPLACED FOR DISEASE AND INSECT INFESTATION AND MAINTENANCE WILL ADDRESS MULCH MATERIAL AND PRUNING.
- SCHEDULE OF PLANT INSPECTION WILL BE ONCE A YEAR IN SPRING AND FALL. THIS INSPECTION WILL INCLUDE REMOVAL OF DISEASED VEGETATION. CONSIDER BEYOND TREATMENT TREATMENT OF ALL DISEASED TREES AND SHRUBS AND REPLACEMENT OF ALL DEFICIENT STAKES AND WIRES.
- MULCH SHALL BE INSPECTED EACH SPRING. REMOVE PREVIOUS MULCH LAYER BEFORE APPLYING NEW LAYER ONCE EVERY 2 TO 3 YEARS.
- SOIL EROSION TO BE ADDRESSED ON AN AS NEEDED BASIS, WITH A MINIMUM OF ONCE PER MONTH AND AFTER HEAVY STORM EVENTS.

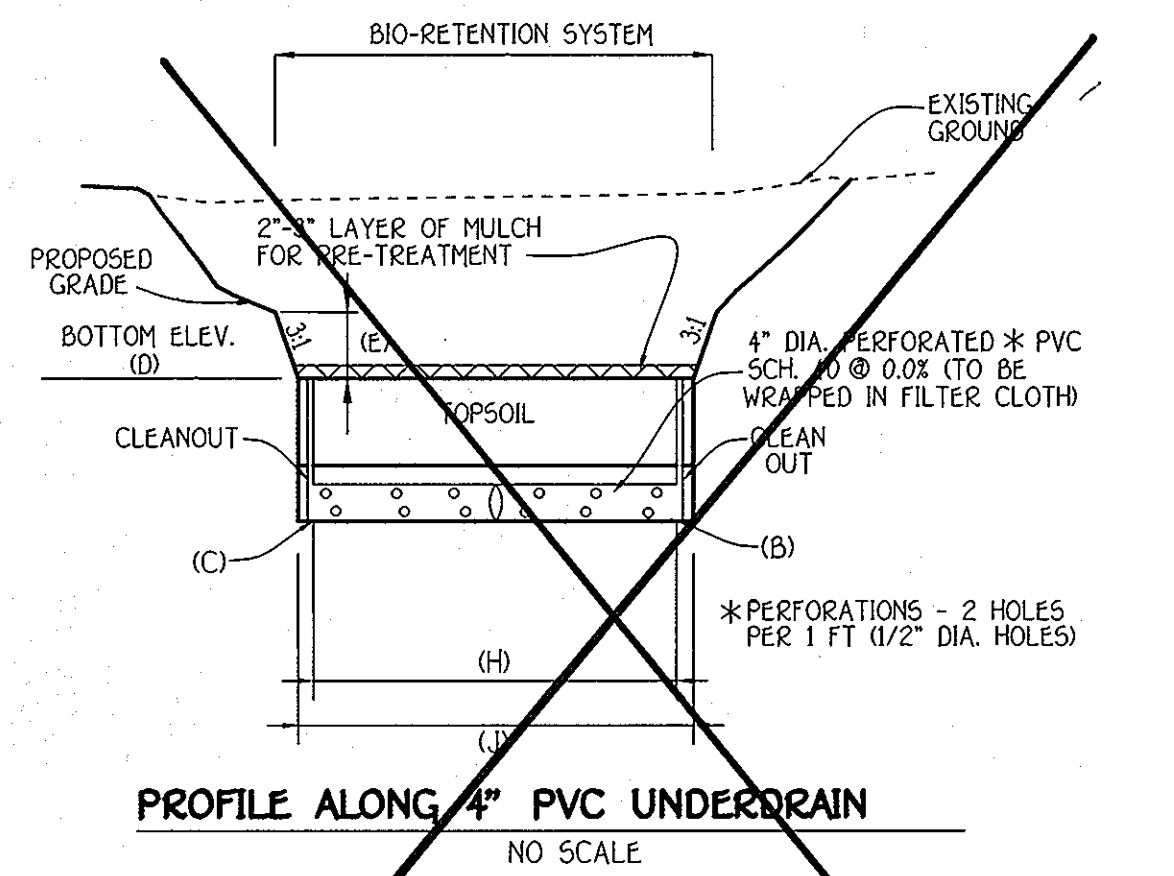
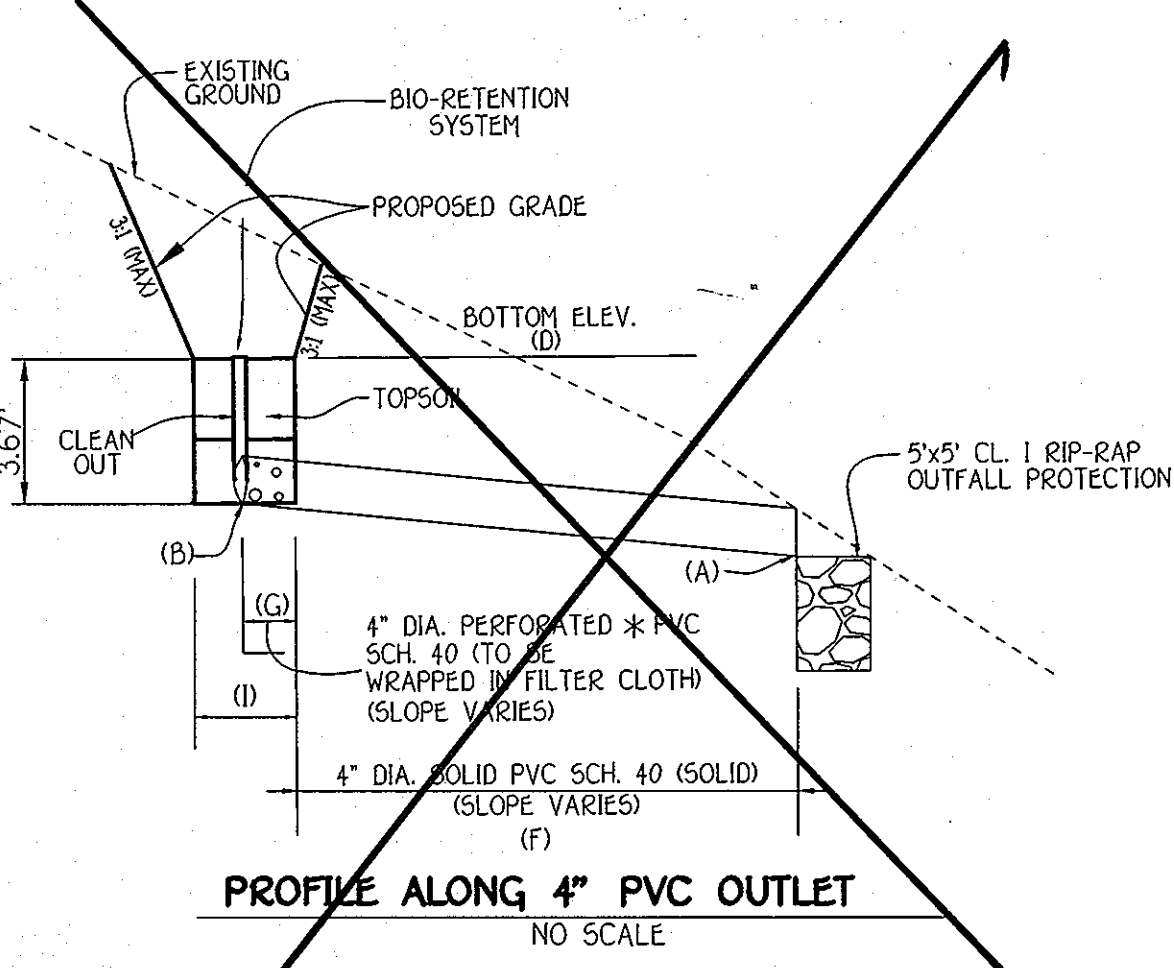
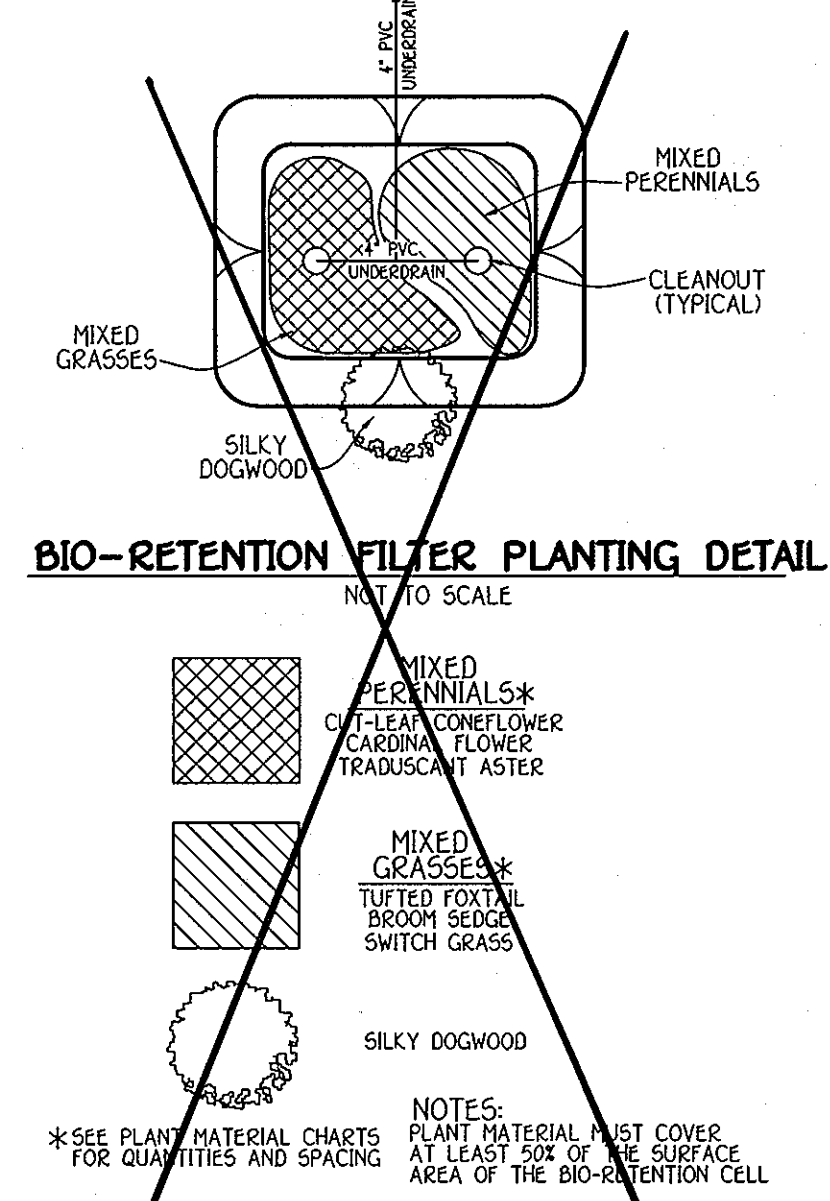
Table B.3.2 Materials Specifications for Bio-retention

Material	Specification	Size	Notes
Plantings	see Appendix A, Table A.4	n/a	plantings are site-specific
planting soil (2.5' to 4' deep)	sand 35-60% silt 30-55% clay 10-25%	n/a	USDA soil types loamy sand, sandy loam or loam
mulch	shredded hardwood		aged 6 months, minimum
pea gravel diaphragm and curtain drain	pea gravel ASTM-D-448 ornamental stone washed cobbles	pea gravel No. 6 stone: 2" to 5"	
geotextile	Class "C" - apparent opening size (ASTM-D-4751) grab tensile strength (ASTM-D-4532), puncture resistance (ASTM-D-4833)	n/a	for use as necessary beneath underdrains only
underdrain gravel	AASHTO M-43	0.375" to 0.75"	
underdrain piping	F 750, Type P5 20 or AASHTO M-278	4" to 6" rigid schedule 40 PVC or SDR35	3/8" perf. @ 6" on center, 4 holes per row; minimum of 3" of gravel over pipes; not necessary underneath pipes
poured in place concrete (if required)	MSHA, Mix No. 3, f'c= 3500 psi @ 28 days, normal weight, air-entrained reinforcing to meet ASTM-615	n/a	on-site testing of poured-in-place concrete required: 28 day strength and slump test; all concrete design (cast-in-place or pre-cast) must use previously approved state or local standards requires design drawings sealed and approved by a professional structural engineer licensed in the State of Maryland design to include meeting ACI Code 308.2R/89; vertical loading 0H-10 or H-20; allowable horizontal loading (based on soil pressures); and analysis of potential cracking
Sand (1' deep)	AASHTO M-6 or ASTM-C-33	0.02" to 0.04"	Sand substitutions such as Diabase and Gyrstone (M) are not acceptable. No calcium carbonated or dolomitic sand substitutions are acceptable. No "rock dust" can be used for Sand.



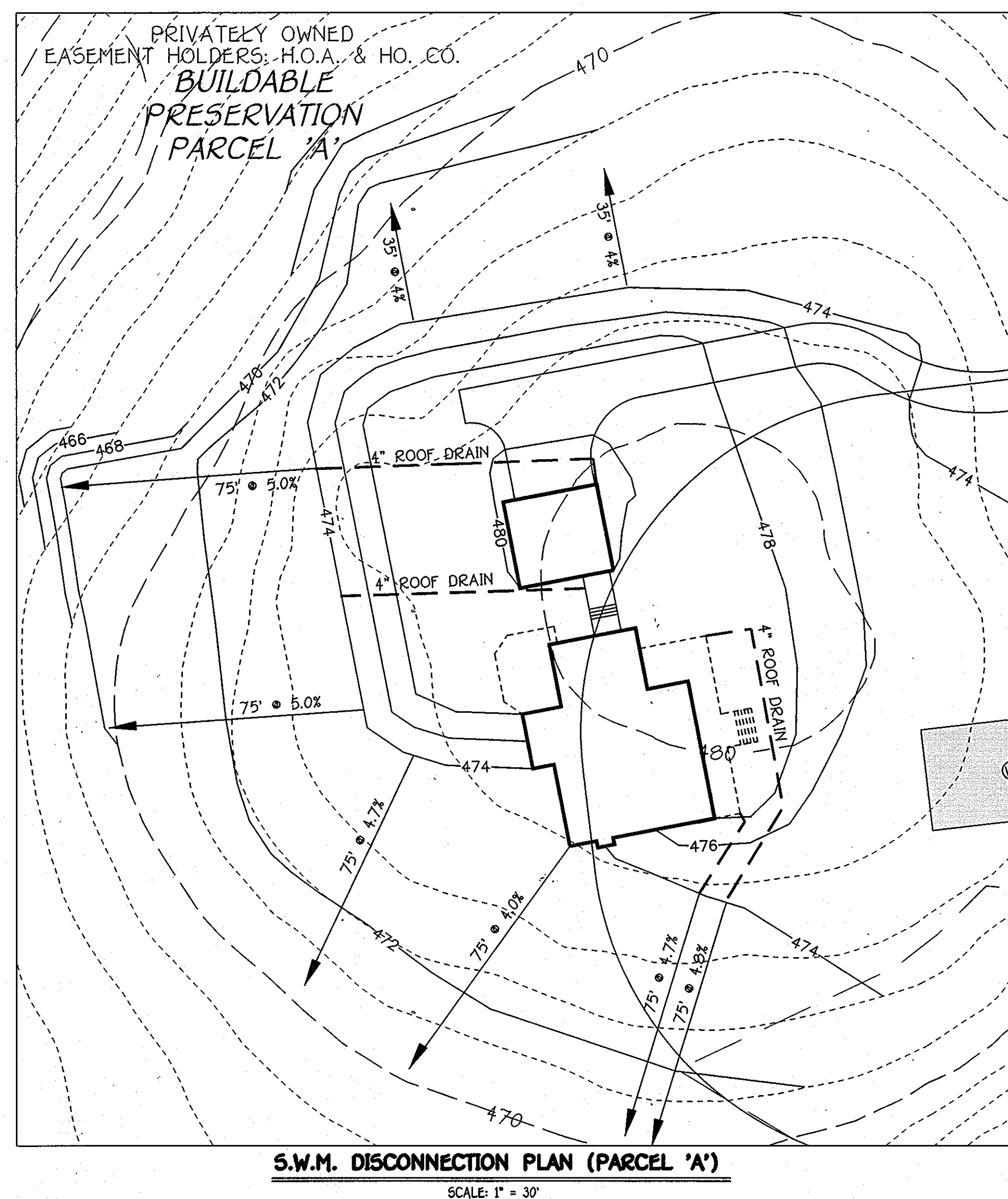
**BIO-RETENTION No. 1 PLANT MATERIAL**

QUANTITY	NAME	MAXIMUM SPACING (FT.)
30	PERENNIALS	1 FT.
30	MIXED GRASSES	1 FT.
1	SILKY DOGWOOD	PLANT AWAY FROM INLET LOCATION



**BIO-RETENTION FILTER DATA**

BIO-RETENTION FILTER	A	B	C	D	E	F	G	H	I	J
BMP #1	465.0	469.08	469.08	473.0	474.0	4'	2.5'	39.0'	5.0'	45.0'



By The Developer:  
I/We Certify That All Development And/or Construction Will Be Done According To These Plans, And That Any Responsible Personnel Involved In The Construction Project Will Have A Certificate Of Attendance At A Department Of The Environment Approved Training Program For The Control Of Sediment And Erosion Before Beginning The Project. I/We Shall Employ A Registered Professional Engineer To Supervise Pond Construction And Provide The Howard Soil Conservation District With an "As-Built" Plan Of The Pond Within 30 Days Of Completion. I Also Authorize Periodic On-Site Inspections By The Howard Soil Conservation District.

Signature of Developer: Robert Goodale  
Date: 12-22-05

By The Engineer:  
I Certify That The Plans For Pond Construction, Erosion And Sediment Control Represent A Practical And Feasible Solution To The Requirements Of The Site Conditions. This Plan Was Prepared In Accordance With The Requirements Of The Howard Soil Conservation District. I Have Notified The Developer, That Should They Engage A Registered Professional Engineer To Supervise Pond Construction, That The Howard Soil Conservation District With an "As-Built" Plan Of The Pond Within 30 Days Of Completion.

Signature of Engineer: [Signature]  
Date: 12-22-09

Approved Department of Public Works  
Signature: [Signature]  
Date: 1-22-09

Approved Department of Planning and Zoning  
Signature: [Signature]  
Date: 2/5/09

Signature of Chief, Development Engineering Division: [Signature]  
Date: 2/4/09

AS-BUILT CERTIFICATION  
I hereby certify that the facility shown on this plan was constructed as shown on the "As-Built" Plans and Meets the Approved Plans and Specifications.

Signature: \_\_\_\_\_ P.E. No. \_\_\_\_\_  
Date: \_\_\_\_\_

Certify Plans To State Or Declare A Professional Opinion Based Upon Onsite Inspections And Material Tests Which Are Conducted During Construction. The Onsite Inspections And Material Tests Are Those Inspections And Tests Deemed Sufficient And Appropriate. I/We Accept Engineering Standards, Certify Our Own Plans And Specifications, And Assume All Liability For Our Own Plans And Specifications. I/We Assume All Liability For Our Own Plans And Specifications. I/We Assume All Liability For Our Own Plans And Specifications. I/We Assume All Liability For Our Own Plans And Specifications.

NO.	DESCRIPTION	DATE
1	REVISED PRIVATE STORMWATER MANAGEMENT FOR PARCEL 'A' PER GP-11-009-A	9/13/12
	REVISIONS	

NOTE: STORMWATER MANAGEMENT SHOWN HEREON IS TAKEN FROM GP-11-009-A.

PRIVATE BIO-RETENTION B.M.P. No. 1  
PLAN AND DETAILS  
**MERIWETHER FARM**  
SECTION ONE  
LOTS 1 THRU 12, BUILDABLE PRESERVATION PARCEL 'A',  
NON-BUILDABLE PRESERVATION PARCELS 'B' THRU 'D'  
AND NON-BUILDABLE PARCEL 'E'

ZONE: RC-DEO  
TAX MAP No. 21, GRID No. 14, 20 & 21 PARCEL No. 24  
FOURTH ELECTION DISTRICT, HOWARD COUNTY, MARYLAND  
DATE: DECEMBER 17, 2008  
SHEET 20 OF 20

**FISHER, COLLINS & CARTER, INC.**  
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**DEVELOPER**  
HERITAGE LAND DEVELOPMENT  
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LISBON, MARYLAND 21765  
(410) 489-7900

Professional Engineer  
I/We hereby certify that these documents were prepared by me, and that I am a duly Licensed Professional Engineer under the laws of the State of Maryland, License No. 20748, Expiration Date 2-22-09.

Signature: [Signature]  
Date: 12-22-09

12/20/09 04:05:00 PM PHASE ONE FINAL/04055 SHEET 20 BIOTENTION PLAN & DETAILS/09 11:20:03 AM jrm/