

Maryland Historical Trust

Maryland Inventory of Historic Properties number: HO 655

Name: HO 110 / Henryton Rd over Patuxent Riv. Trib

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridge received the following determination of eligibility.

MARYLAND HISTORICAL TRUST	
Eligibility Recommended _____	Eligibility Not Recommended <u>X</u>
Criteria: <u> </u> A <u> </u> B <u> </u> C <u> </u> D	Considerations: <u> </u> A <u> </u> B <u> </u> C <u> </u> D <u> </u> E <u> </u> F <u> </u> G <u> </u> None
Comments: _____ _____ _____	
Reviewer, OPS: <u>Anne E. Bruder</u>	Date: <u>3 April 2001</u>
Reviewer, NR Program: <u>Peter E. Kurtze</u>	Date: <u>3 April 2001</u>

MARYLAND INVENTORY OF HISTORIC BRIDGES
HISTORIC BRIDGE INVENTORY
MARYLAND STATE HIGHWAY ADMINISTRATION/
MARYLAND HISTORICAL TRUST

MHT No. HO-655

SHA Bridge No. HO 110 Bridge name Henryton Road over Patapsco River Tributary

LOCATION:

Street/Road name and number [facility carried] Henryton Road

City/town Sykesville Vicinity X

County Howard

This bridge projects over: Road Railway Water Land

Ownership: State County Municipal Other

HISTORIC STATUS:

Is the bridge located within a designated historic district? Yes No

National Register-listed district National Register-determined-eligible district

Locally-designated district Other

Name of district _____

BRIDGE TYPE:

Timber Bridge _____:

Beam Bridge Truss -Covered Trestle Timber-And-Concrete

Stone Arch Bridge _____

Metal Truss Bridge _____

Movable Bridge _____:

Swing Bascule Single Leaf Bascule Multiple Leaf

Vertical Lift Retractable Pontoon

Metal Girder _____:

Rolled Girder Rolled Girder Concrete Encased

Plate Girder Plate Girder Concrete Encased

Metal Suspension _____

Metal Arch _____

Metal Cantilever _____

Concrete :

Concrete Arch Concrete Slab Concrete Beam Rigid Frame

Other Type Name _____

DESCRIPTION:

Setting: Urban _____ Small town _____ Rural X

Describe Setting: Bridge No. HO 110 carries Henryton Road over Patapsco River Tributary in Howard County. Henryton Road runs north-south, while the tributary of the Patapsco River flows from the east to the west. The area around the bridge is forested with a few buildings within the vicinity of the bridge.

Describe Superstructure and Substructure:

Bridge No. HO 110 over a Tributary of the Patapsco River in Howard County is a single span concrete slab bridge built c. 1930. The clear span length is 19'-6", the total bridge length is 21' and the clear roadway width is 27'. The bridge is not currently posted. The bridge is skewed at a 16 degree angle to the roadway centerline. The superstructure consists of the roadway, slab and railing. The roadway surface has a large patch on the east side of the bridge roadway. The slab has approximately 4' of fill between the concrete deck and the road surface. The west fascia has longitudinal cracking, while the east fascia has areas of spalling and efflorescence at the deck/curb intersection. This area has been previously patched. The parapets were replaced with w-beam guardrails at an unknown date. The substructure consists of the concrete abutments and wingwalls. The west wingwalls are parallel with the abutment center line, while the east abutments are flared.

Discuss Major Alterations:

Four feet of fill was placed on top of the slab and the concrete parapets were replaced with w-beam guardrails. Both alterations occurred at an unknown date.

HISTORY:

WHEN was the bridge built: 1930

This date is: Actual _____ Estimated X

Source of date: Plaque _____ Design plans _____ County bridge files/inspection form X

Other (specify)

WHY was the bridge built?

Maryland's primary and secondary roads and bridges had become inadequate to the huge trucks and volumes of cars in use after World War I.

WHO was the designer?

Unknown

WHO was the builder?

Unknown

WHY was the bridge altered?

The bridge was altered to extend the life of the bridge.

Was this bridge built as part of an organized bridge-building campaign?

Yes, post World War I improvements to primary and secondary roads.

SURVEYOR/HISTORIAN ANALYSIS:

This bridge may have National Register significance for its association with:

- A - Events _____
- B- Person _____
- C- Engineering/architectural character _____

The bridge does not have National Register significance.

Was the bridge constructed in response to significant events in Maryland or local history?

Maryland's roads and bridge improvement programs mirrored economic cycles. The first road improvement of the State Roads Commission was a 7 year program, starting with the Commissions establishment in 1908 and ending in 1915. Due to World War I, the period from 1916-1920 was one of relative inactivity; only roads of first priority were built. Truck traffic resulting from war related factories and military installations generated new, heavy traffic unanticipated by the builders of the early road system. From 1920-1929, numerous highway improvements occurred in response to the increase in Maryland motor vehicles from 103,000 in 1920 to 320,000 in 1929, with emphasis on the secondary system of feeder roads which moved traffic from the primary roads built before World War I. After World War I, Maryland's bridge system also was appraised as too narrow and structurally inadequate for the increasing traffic, with plans for an expanded bridge program to be handled by the Bridge Division, set up in 1920. In 1920 under Chapter 508 of the Acts of 1920 the State issued a bond of \$3,000,000.00 for road construction; the primary purpose of these monies was to meet the state obligations involving the construction of rural post roads. The secondary purpose of these monies was to fund (with an equal sum from the counties) the building of lateral roads. the number of hard surfaced roads on the state system grew from 2000 in 1920 to 3200 in 1930. By 1930, Maryland's primary system had been inadequate to the huge freight trucks and volume of passenger cars in use, with major improvements occurring in the late 1930's. Most improvements to local roads waited until the years after World War II.

When the bridge was built and/or given a major alteration, did it have a significant impact on the growth and development of the area?

No, this bridge did not have a direct impact on the growth or development of the area.

Is the bridge located in an area which may be eligible for historic designation and would the bridge add to or detract from the historic/visual character of the potential district?

No, the bridge is not located in an area which may be eligible for historic designation.

Is the bridge a significant example of its type?

No, this bridge is not a significant example of its type because its character defining features have been altered or they are in a deteriorated state.

Does the bridge retain integrity of important elements described in Context Addendum?

No, this structure does not retain the integrity of its original design because the parapets were removed.

Is the bridge a significant example of the work of a manufacturer, designer, and/or engineer?

Unknown

Should the bridge be given further study before an evaluation of its significance is made?

No, this structure should not be given further study. Its current condition has placed its integrity in doubt.

BIBLIOGRAPHY:

County inspection/bridge files X SHA inspection/bridge files
Other (list):

SURVEYOR:

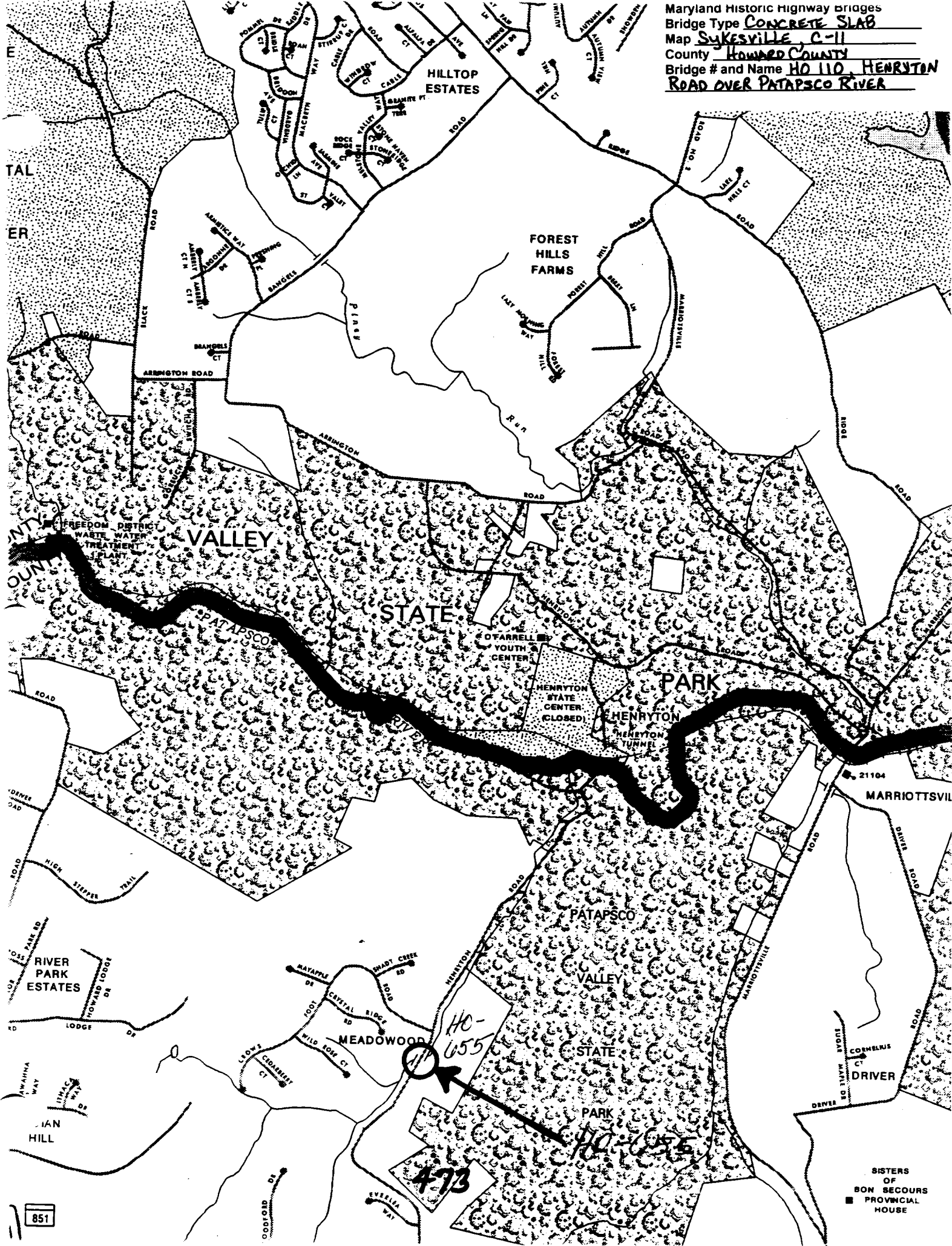
Date bridge recorded 8/95

Name of surveyor Leo Hirrell

Organization/Address P.A.C. Spero & Company, 40 W. Chesapeake Avenue, Suite 412, Baltimore, MD 21204

Phone number (410) 296-1635 FAX number (410) 296-1670

Maryland Historic Highway Bridges
 Bridge Type CONCRETE SLAB
 Map Sykesville, C-11
 County HOWARD COUNTY
 Bridge # and Name HO 110, HENRYTON ROAD OVER PATAPSCO RIVER



851

SISTERS OF BON SECOURS PROVINCIAL HOUSE



Inventory # H0-655

Name H0110-HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description WEST APPROACH LOOKING EAST
^{SOUTH} ^{NORTH}

Number ¹ 8 of ⁴ 83



Inventory # 110-655

Name 10110-HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description EAST
SOUTH ELEVATION LOOKING

NORTHWEST

Number 2 of 4
9 of 33

10-11-1995



Inventory # HO-655

Name HO110-HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description WEST
NORTH ELEVATION LOOKING

SOUTHWEST

Number 3 of 4
~~10~~ of ~~35~~



Inventory # HO-655

Name HO 110 - HENRYTON RD OVER STREAM

County/State HOWARD / MD

Name of Photographer DAVID DIEHL

Date 2/95

Location of Negative SHA

Description NORTH
EAST APPROACH LOOKING

WEST
SOUTH

Number 4 of 4
33

NO. 40 STATE PERMIT 1109